



**City of Phoenix**  
PLANNING AND DEVELOPMENT DEPARTMENT

**Staff Report Z-165-25-8**  
April 10, 2026

[Laveen Village Planning Committee](#) Meeting Date: April 13, 2026

[Planning Commission](#) Hearing Date: May 7, 2026

**Request From:** [GC](#) (Golf Course District) (4.35 acres)  
**Request To:** [C-2](#) (Intermediate Commercial) (4.35 acres)  
**Proposal:** General retail and restaurant  
**Location:** Southwest corner of 55th Avenue and Southern Avenue  
**Owner:** 55 South 3, LLC  
**Applicant:** Alex Hayes, Withey Morris Baugh, PLC  
**Representative:** Adam Baugh, Withey Morris Baugh, PLC  
**Staff Recommendation:** Approval, subject to stipulations

<a href="#">General Plan Conformity</a>			
<a href="#">General Plan Land Use Map Designation</a>		Parks / Open Space-Private and Residential 3.5 to 5 dwelling units per acre	
<a href="#">Street Map Classification</a>	Southern Avenue	Arterial	55-foot south half street
	55th Avenue	Minor Collector	+30-foot west half street
	Cottonfields Lane	Private	20 to +57-foot east half street
<p><b><i>STRENGTHEN OUR LOCAL ECONOMY CORE VALUE; ENTREPRENEURS AND EMERGING ENTERPRISES: Encourage land uses that promote the growth of entrepreneurs or new businesses in Phoenix in appropriate locations.</i></b></p> <p>The proposal will develop vacant land and will provide additional commercial development along an arterial street.</p>			

**CONNECT PEOPLE AND PLACES CORE VALUE; OPPORTUNITY SITES; LAND USE PRINCIPLE: Support reasonable levels of increased intensity, respectful of local conditions and surrounding neighborhoods.**

The proposal, as stipulated, provides a reasonable level of intensity that is respectful to local conditions by providing buildings with modern agrarian architectural features which reflects Laveen's preference for a rural aesthetic, and large caliper trees within the perimeter landscape setback adjacent to single-family residential.

**BUILD THE SUSTAINABLE DESERT CITY CORE VALUE; TREES AND SHADE; DESIGN PRINCIPLE: Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.**

The proposal, as stipulated, includes perimeter landscaping and shaded surface parking. This will create a comfortable pedestrian environment, reduce the urban heat island affect, and make the walk to nearby destinations safer and more comfortable. In addition, a stipulation has been recommended for enhanced landscaping within the landscape setbacks that are not adjacent to streets.

#### Applicable Plans, Overlays, and Initiatives

[Laveen Southwest Growth Study](#): Background Item No. 6.

[Complete Streets Guiding Principles](#): Background Item No. 7.

[Comprehensive Bicycle Master Plan](#): Background Item No. 8.

[Shade Phoenix Plan](#): Background Item No. 9.

[Transportation Electrification Action Plan](#): Background Item No. 10.

[Conservation Measures for New Development](#): Background Item No. 11.

[Phoenix Climate Action Plan](#): Background Item No. 12.

[Zero Waste PHX](#): Background Item No. 13.

<b>Surrounding Land Uses/Zoning</b>		
	<b><u>Land Use</u></b>	<b><u>Zoning</u></b>
<b>On Site</b>	Vacant land	GC
<b>North (across Southern Avenue)</b>	Single-family residential	R1-8
<b>East (across 55th Avenue)</b>	Single-family residential	R1-8
<b>South</b>	Vacant land (proposed single-family)	PUD
<b>West (across Cottonfields Lane)</b>	Vacant land (proposed single-family)	PUD

<b>C-2 (Intermediate Commercial)</b>		
<b><u>Standards</u></b>	<b><u>Requirements</u></b>	<b><u>Provisions on the Proposed site Plan</u></b>
<b><i>Minimum Building Setbacks</i></b>		
North (adjacent to Southern Avenue)	Average 25 feet, minimum 20 feet permitted for up to 50% of the structure	25 feet (Met)
East (adjacent to 55th Avenue)	Average 25 feet, minimum 20 feet permitted for up to 50% of the structure	Approximately 56 feet (Met)
South and southeast (adjacent to PUD and R1-8)	1 story (or 15 feet): 25 feet 2-story (or 30 feet): 50 feet	35 feet (Met)
West (adjacent to Cottonfields Lane)	Average 25 feet, minimum 20 feet permitted for up to 50% of the structure	50 feet (Met)
<b><i>Minimum Landscape Setbacks</i></b>		
North (adjacent to Southern Avenue)	Average 25 feet, minimum 20 feet for up to 50% of the frontage	Average 25 feet, minimum 20 feet (Met)
East (adjacent to 55th Avenue)	Average 25 feet, minimum 20 feet for up to 50% of the frontage	Average 25 feet, minimum 20 feet (Met)
South and southeast (adjacent to PUD and R1-8)	10 feet	10 feet (Met)
West (adjacent to Cottonfields Lane)	Average 25 feet, minimum 20 feet for up to 50% of the frontage	Average 25 feet, minimum 20 feet (Met)

<b>C-2 (Intermediate Commercial)</b>		
Maximum Building Height	2 stories, 30 feet	15 feet (Met)
Maximum Lot coverage	50 percent	15.2 percent (Met)
Minimum Parking	132 spaces; General retail: 1 space per 300 square feet Restaurant with outdoor seating: 1 space per 50 square feet	143 spaces provided (Met)

**Background/Issues/Analysis**

**SUBJECT SITE**

1. This request is to rezone 4.35 acres located at the southwest corner of 55th Avenue and Southern Avenue from GC (Golf Course District) to C-2 (Intermediate Commercial) to allow general retail and restaurant.

**SURROUNDING LAND USES AND ZONING**

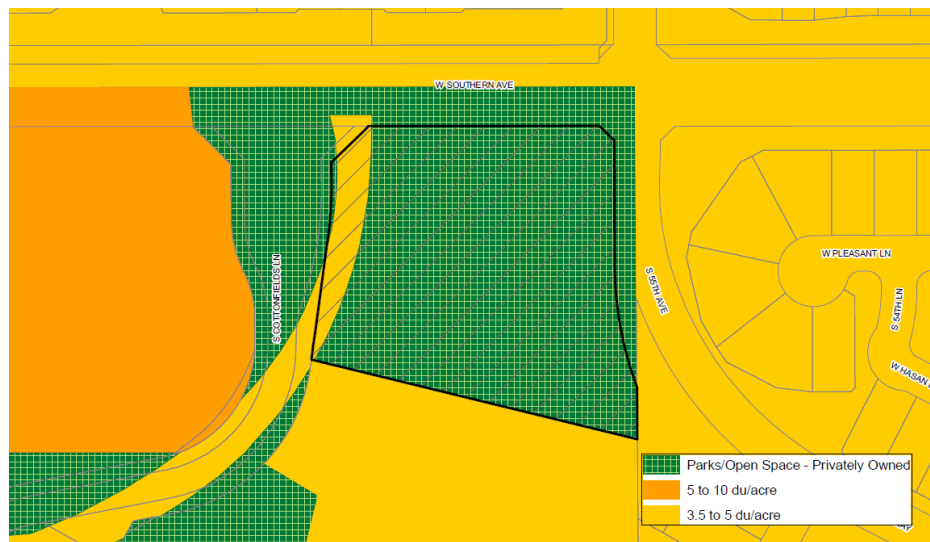
2. North of the subject site, across Southern Avenue, is a single-family subdivision zoned R1-8 (Single-Family Residence District). The area to the east, across 55th Avenue, is a single-family subdivision zoned R1-8 (Single-Family Residence District). The areas to the west, across 55th Avenue, and south are currently vacant, previously developed as a golf course, and recently rezoned to PUD (Planned Unit Development). The proposed PUD will include a single-family development with a revitalized golf course.



Existing Zoning Aerial Map  
Source: Planning and Development Department

### GENERAL PLAN LAND USE MAP DESIGNATION

3. The subject site is designated mostly Parks / Open Space – Privately Owned with some Residential 3.5 to 5 dwelling units per acre along the private street on the General Plan Land Use Map. The property to the east and south is designated Residential 3.5 to 5 dwelling units per acre. North of the subject site, across Southern Avenue is designated Residential 3.5 to 5 dwelling units per acre. The vacant property west of the subject site, across Cottonfields Lane, is designated Residential 10 to 15 dwelling units per acre and Parks/Open Space – Privately Owned. The proposed C-2 zoning district is not consistent with the General Plan Land Use Map designation; however, a General Plan Amendment is not required because the site is under 10 acres in size.



General Plan Land Use Map  
Source: Planning and Development Department

### PROPOSAL

#### 4. **Site Plan**

The conceptual site plan, attached as an exhibit, depicts two approximately 10,000-square foot retail buildings located along the south and west portion of the site. A smaller 3,000-square foot pad is located along Southern Avenue. The subject site has vehicle access off Southern Avenue and 55th Avenue. In order to buffer future residential development to the south, staff recommends two-inch caliper, large canopy, drought-tolerant, shade trees to be planted along the perimeter landscape setbacks. This is addressed in Stipulation No. 4. The proposed development meets all minimum development standards in the Zoning Ordinance; as a result, staff recommends general conformance to the conceptual site plan. This is addressed in Stipulation No. 1.



*Proposed Site Plan  
Source: Norris Design*

5. **Elevations**

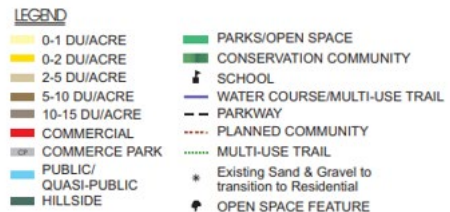
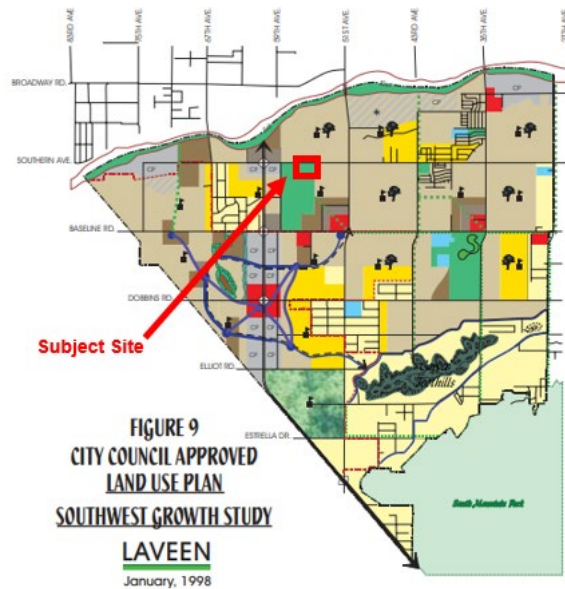
The applicant has not submitted elevations for the proposed commercial or restaurant development. However, they have submitted character images for future development. As a result, staff recommends any proposed elevations to be reviewed by the Laveen Village Planning Committee prior to preliminary site plan approval. This is addressed in Stipulation No. 2. Furthermore, all perimeter street facing building elevations shall contain architectural features that reflect modern agrarian architecture with pitched roofs, variation in window sizes, and accent materials. This is addressed in Stipulation No. 3.

## PLANS, OVERLAYS, AND INITIATIVES

### 6. [Laveen Southwest Growth Study:](#)

The site is located within the boundaries of the Laveen Southwest Growth Study, which was developed in 1997 to analyze the existing conditions of the Laveen Village and provide a land use and design planning framework to help shape the growth that Laveen was starting to experience, while accounting for newly annexed farmland as well as the future development of the South Mountain Freeway Loop, which has since been completed. This plan designates the project site as Parks/Open Space. Although not consistent with the designation, recent land use trends in the area, including the redevelopment of the golf course, support the proposed development. The Laveen Southwest Growth Study also

outlines specific design policies and standards for various types of development that will enhance Laveen's built environment while remaining respectful to its agricultural heritage. The study encourages all new development to use durable, high quality building materials and to provide enhanced building design that will contribute to the character of the area. The proposal incorporates accent materials and features that will be compatible with the agrarian culture of the Laveen Village, per Stipulation No. 3.



### 7. [Complete Streets Guiding Principles](#)

In 2014, the City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles. To provide a safe pedestrian environment, staff recommends enhanced pavement treatment for the on-site pedestrian walkways that cross vehicular drive aisles. This is addressed in Stipulation No. 6. Furthermore, a pedestrian pathway shall be provided along Cottonfields Lane to connect the proposed commercial development. This is addressed in Stipulation No. 5. Stipulation No. 13 will help pedestrianize the immediate street frontage by requiring a detached sidewalk with landscaping along Southern Avenue.

8. **Comprehensive Bicycle Master Plan**

The City of Phoenix adopted the Comprehensive Bicycle Master Plan in 2014 to guide the development of its Bikeway System and supportive infrastructure. The Comprehensive Bicycle Master Plan supports options for both short- and long-term bicycle parking as a means of promoting bicyclist traffic to a variety of destinations. As stipulated, the proposal will provide bicycle parking per the City's Walkable Urban Code. This is addressed in Stipulation No. 8.

9. **Shade Phoenix Plan**

In November 2024, the Phoenix City Council adopted the Shade Phoenix Plan. The Shade Phoenix Plan prioritizes increasing shade coverage throughout the City to improve health and quality of life. Investing in shade can address the urban heat island effect, clean the air, preserve Sonoran vegetation, and prevent health complications related to prolonged exposure to heat. The Shade Phoenix Plan provides numerous strategies to increase shade including expanding and maintaining existing shade, strengthening tree code enforcement, and developing shade stipulations in rezoning cases. Staff recommends stipulations designed to provide trees and enhanced shade within and adjacent to the development as follows:

- The surface parking lot area shall be landscaped with minimum two-inch, drought tolerant, shade trees or shade structure/s to achieve a minimum of 15 percent shade (Stipulation No. 7);
- The perimeter landscape setback, not adjacent to a street, shall be planted with minimum two-inch caliper, large canopy, drought-tolerant, shade trees (Stipulation No. 4);
- A detached sidewalk with a 6-foot-wide landscape area planted with two-inch caliper, single-trunk, large canopy, drought-tolerant shade trees along Southern Avenue (Stipulation No. 13);

10. **Transportation Electrification Action Plan**

In June 2022, the Phoenix City Council approved the Transportation Electrification Action Plan. The current market desire for the electrification of transportation is both a national and global phenomenon, fueled by a desire for better air quality, a reduction in carbon emissions, and a reduction in vehicle operating and maintenance costs. Businesses, governments and the public are signaling strong future demand for electric vehicles (EVs), and many automobile manufacturers have declared plans for a transition to fully electric offerings within the coming decade. This Plan contains policy initiatives to prepare the City for a future filled with more EVs, charging infrastructure and e-mobility equity, and outlines a roadmap for a five-step plan to prepare for the EV infrastructure needs of 280,000 EVs in Phoenix by 2030. One goal of the Plan to accelerate public adoption of electric vehicles through workplace, business, and multifamily

charging infrastructure recommends a standard stipulation for rezoning cases to provide EV charging infrastructure. This is addressed in Stipulation No. 11.

11. **Conservation Measures for New Development**

In June 2023, the Phoenix City Council adopted the Conservation Measures for New Development policy as part of a resolution addressing the future water consumption of new development (Resolution 22129). This resolution addresses the future water consumption of new development to support one of the City's Five Core Values in the General Plan which calls for Phoenix to - *Build the Sustainable Desert City*. The Conservation Measures for New Development policy includes direction to develop standards for consideration as stipulations for all rezoning cases that will address best practices related to water usage in nine specific categories. This is addressed in Stipulation Nos. 7, 9, and 10.

12. **Phoenix Climate Action Plan**

In October 2021, the Phoenix City Council approved the Climate Action Plan. The Climate Action Plan will serve as a long-term plan to achieve greenhouse gas emissions reductions and resiliency goals from local operations and community activities as well as prepare for the impacts of climate change. This plan contains policy and initiatives regarding stationary energy, transportation, waste management, air quality, local food systems, heat, and water. Goal W2 (Water), Action W2.4, pertains to the implementation of the [Greater Phoenix Green Infrastructure \(GI\) and Low Impact Development Details for Alternative Stormwater Management](#) to benefit the environment, promote water conservation, reduce urban heat, improve the public health, and create additional green spaces. This goal is addressed in Stipulation No. 9, which requires a minimum of two GI techniques for stormwater management to be implemented if the site is ever redeveloped.

13. **Zero Waste PHX:**

The City of Phoenix is committed to its waste diversion efforts and has set a goal to become a zero waste city, as part of the city's overall 2050 Environmental Sustainability Goals. One of the ways Phoenix can achieve this is to improve and expand its recycling and other waste diversion programs. Section 716 of the Phoenix Zoning Ordinance establishes standards to encourage the provision of recycling containers for multifamily, commercial, and mixed-use developments meeting certain criteria. The development narrative states that recycling services will be reviewed during the site planning process.

COMMUNITY INPUT SUMMARY

14. As of the writing of this report, staff has received one letter of opposition and one letter of support for this rezoning case. Some of the stated concerns included the proposed use and the removal of golf course property.

#### INTERDEPARTMENTAL COMMENTS

15. The Street Transportation Department has requested the following:
- The south half of Southern Avenue shall be constructed to an Arterial CM cross-section.
  - A 6-foot-wide detached sidewalk with a minimum 6-foot-wide landscape area shall be constructed on the south side of Southern Avenue.
  - All mitigation improvements shall be funded or constructed as identified by the accepted Traffic Impact Analysis.
  - The existing irrigation facilities along Southern Avenue shall be relocated outside of the City right-of-way.
  - All streets shall be reconstructed/constructed to meet City and ADA standards.

These are addressed in Stipulation Nos. 12 through 17.

#### OTHER

16. The site is located in a larger area identified as being archaeologically sensitive. If further review by the City of Phoenix Archaeology Office determines the site and immediate area to be archaeologically sensitive, and if no previous archaeological projects have been conducted within this project area, it is recommended that archaeological Phase I data testing of this area be conducted. Phase II archaeological data recovery excavations may be necessary based upon the results of the testing. A qualified archaeologist must make this determination in consultation with the City of Phoenix Archaeologist. In the event archaeological materials are encountered during construction, all ground disturbing activities must cease within a 33-foot radius of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. This is addressed in Stipulations Nos. 18 through 20.
17. Staff has not received a completed form for the Waiver of Claims for Diminution in Value of Property under Proposition 207 (A.R.S. 12-1131 et seq.), as required by the rezoning application process. Therefore, a stipulation has been added to require the form be completed and submitted prior to final site plan approval. This is addressed in Stipulation No. 21.
18. Development and use of the site is subject to all applicable codes and ordinances. Zoning approval does not negate other ordinance requirements. Other formal actions such as, but not limited to, zoning adjustments and abandonments, may be required.

## **Findings**

1. The proposal will develop vacant property and provide commercial services adjacent to an arterial street.
2. As stipulated, the proposal supports efforts from various plans and initiatives including the Shade Phoenix Plan, Transportation Electrification Action Plan, and Phoenix Climate Action Plan.
3. The stipulated landscaping planting standards are above the typical requirement and will contribute to making the proposal compatible with the surrounding land uses.

## **Stipulations**

1. The development shall be in general conformance with the site plan date stamped March 19, 2026, as modified by the following stipulations and approved by the Planning and Development Department.
2. Building elevations shall be presented for review and comment to the Laveen Village Planning Committee prior to preliminary site plan approval.
3. All street facing building elevations shall contain architectural features that reflect modern agrarian architecture including, but not limited to, detailing such as pitched roof elements, variation in window size, overhang canopies and exterior accent materials such as metal, wood, and stone, as approved by the Planning and Development Department.
4. The required perimeter landscape setbacks, not adjacent to a street, shall be planted with a minimum 2-inch caliper trees, as approved by the Planning and Development Department.
5. A pedestrian pathway shall be provided along Cottonfields Lane to connect the proposed commercial development to the sidewalk, as approved by the Planning and Development Department and if approval is granted by the adjacent Homeowners Association (owner of the private street/tract).
6. Where pedestrian walkways cross a vehicular path, the pathway shall be constructed of decorative pavers, stamped or colored concrete, or other pavement treatments that visually contrast parking and drive aisle surfaces, as approved by the Planning and Development Department.
7. A minimum of 15% of uncovered surface parking lot areas shall be shaded, as approved by the Planning and Development Department. Shade may be

achieved by structures or by minimum 2-inch caliper, drought tolerant, shade trees, or a combination thereof.

8. Bicycle parking shall be provided, consistent with Section 1307.H of the Phoenix Zoning Ordinance.
9. A minimum of two green stormwater infrastructure (GSI) elements for stormwater management shall be implemented, as approved or modified by the Planning and Development and/or Street Transportation departments. This includes but is not limited to stormwater harvesting basins, bioswales, permeable pavement, etc., per the Greater Phoenix Metro Green Infrastructure and Low Impact Development Details for Alternative Stormwater Management.
10. Prior to final site plan approval, documentation shall be provided that demonstrates a commitment to participate in the City of Phoenix Businesses Water Efficiency Program for a minimum of 10 years, or as approved by the Planning and Development Department.
11. A minimum of 10% of the required vehicle parking spaces shall include EV Ready infrastructure.
12. The south half of Southern Avenue shall be constructed to an Arterial CM cross-section, adjacent to the development.
13. A minimum 6-foot-wide detached sidewalk separated by a minimum 6-foot-wide landscape area shall be constructed on the south side of Southern Avenue, and planted as follows:
  - a. Minimum 2-inch caliper, single-trunk, large canopy, drought-tolerant, shade trees, planted 20 feet on center, or in equivalent groupings.
  - b. Drought-tolerant shrubs, accents and vegetative groundcovers maintained to a maximum height of 3 feet (excluding accents), evenly distributed throughout the landscape area to achieve a minimum of 75% live coverage.

Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.

14. Prior to final site plan approval, all mitigation improvements shall be constructed and/or funded as identified in the accepted Traffic Impact Analysis dated September 26, 2025.

15. Existing irrigation facilities along Southern Avenue shall be relocated outside of City right-of-way, unless otherwise approved by the Street Transportation Department. Relocations that require additional dedications or land transfer require completion prior to obtaining plat and/or civil plan review approval.
16. Replace unused driveways with sidewalk, curb, and gutter. Also, replace any broken or out-of-grade curb, gutter, sidewalk, and curb ramps on all streets and upgrade all off-site improvements to be in compliance with current ADA guidelines.
17. All streets within and adjacent to the development shall be constructed with paving, curb, gutter, sidewalk, curb ramps, streetlights, median islands, landscaping and other incidentals, as per plans approved by the Planning and Development Department. All improvements shall comply with all ADA accessibility standards.
18. If determined necessary by the Phoenix Archaeology Office, the applicant shall conduct Phase I data testing and submit an archaeological survey report of the development area for review and approval by the City Archaeologist prior to clearing and grubbing, landscape salvage, and/or grading approval.
19. If Phase I data testing is required, and if, upon review of the results from the Phase I data testing, the City Archaeologist, in consultation with a qualified archaeologist, determines such data recovery excavations are necessary, the applicant shall conduct Phase II archaeological data recovery excavations.
20. In the event archaeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33-foot radius of the discovery, notify the City Archaeologist, and allow time for the Archaeology Office to properly assess the materials.
21. Prior to final site plan approval, the landowner shall execute a Proposition 207 waiver of claims form. The waiver shall be recorded with the Maricopa County Recorder's Office and delivered to the City to be included in the rezoning application file for record.

**Writer**

Nayeli Sanchez Luna

April 10, 2026

**Team Leader**

Racelle Escobar

Staff Report: Z-165-25-8

April 10, 2026

Page 14 of 14

**Exhibits**

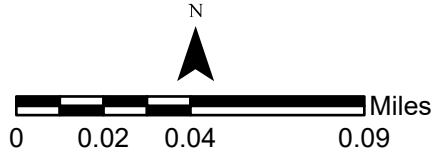
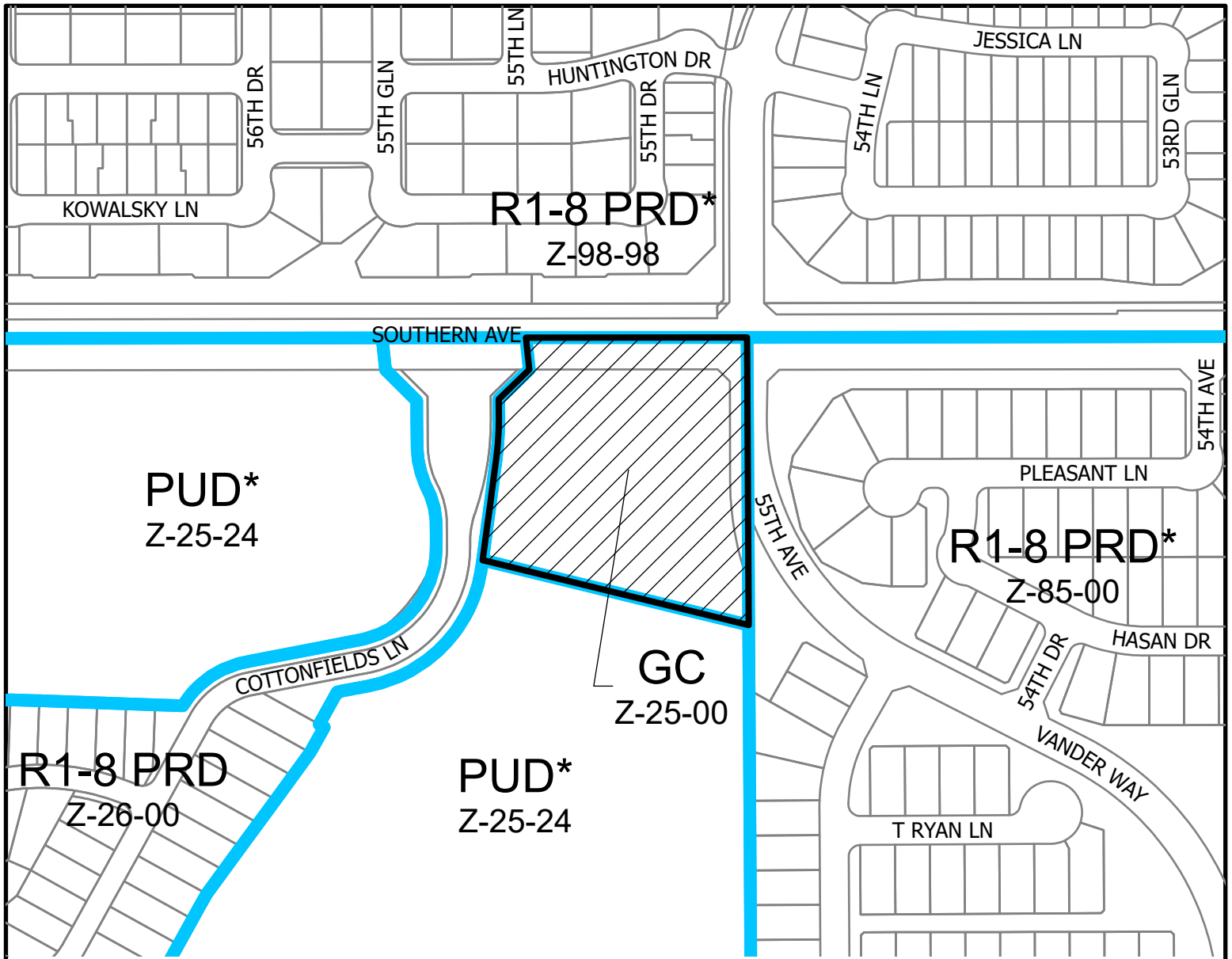
Sketch map

Aerial map

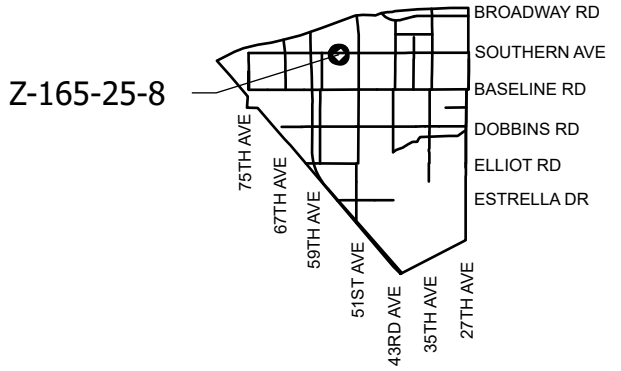
Conceptual site plan date stamped March 19, 2026

Conceptual renderings (character images) date stamped March 19, 2026

Correspondence (2 pages)

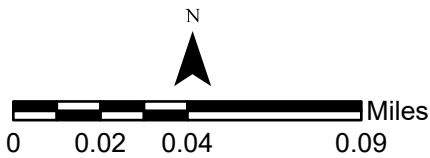
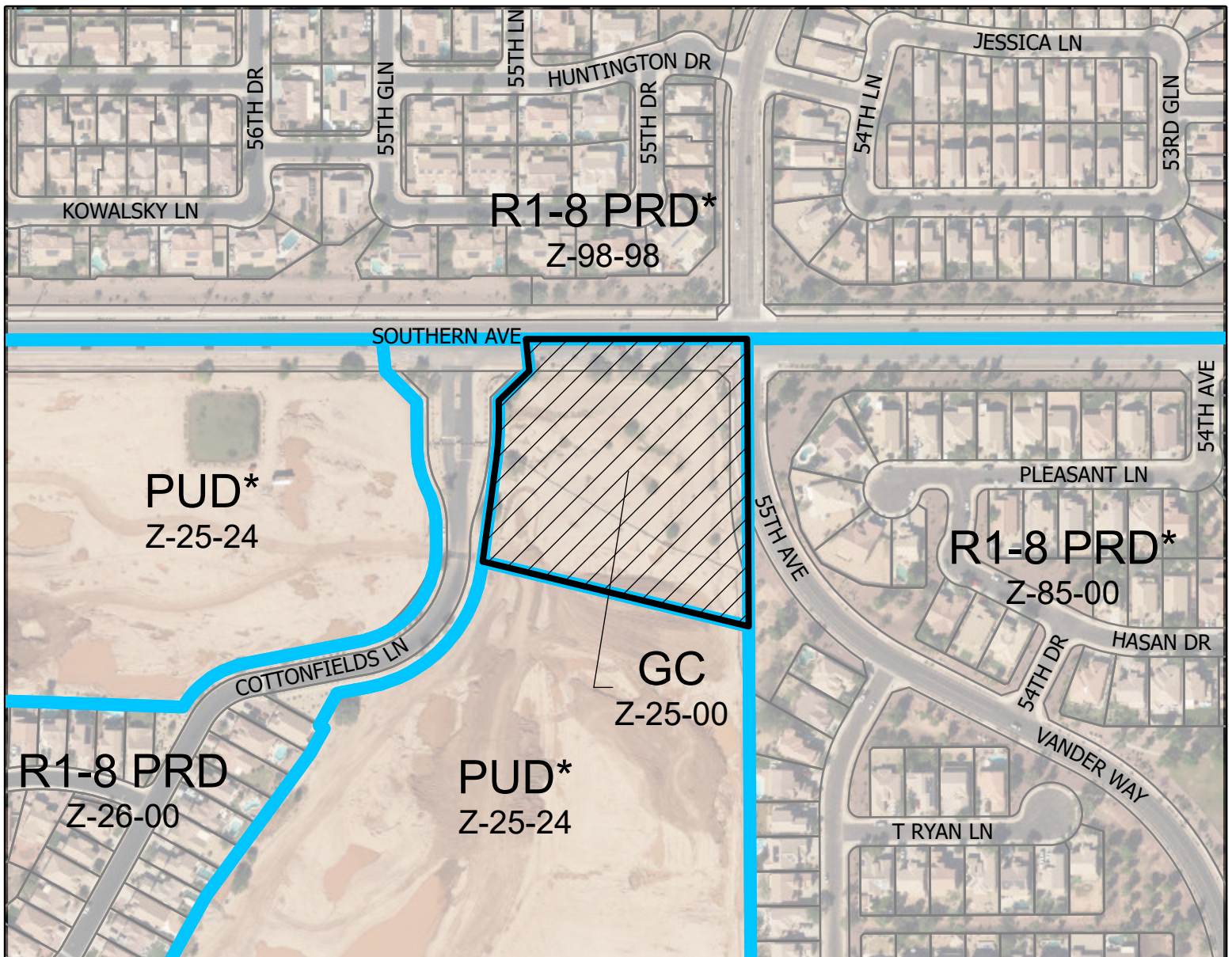


**LAVEEN VILLAGE**  
COUNCIL DISTRICT: 8

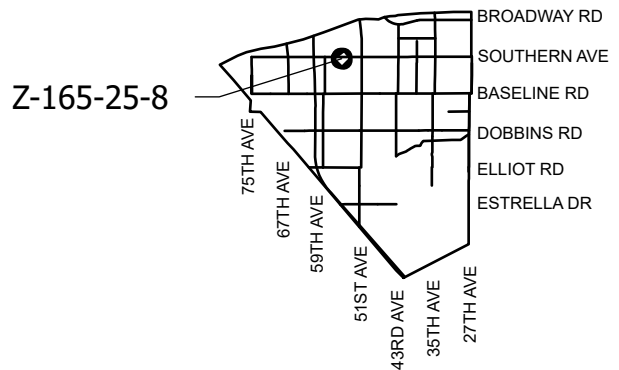


APPLICANT'S NAME: <b>Withey Morris Baugh, PLC</b>		REQUESTED CHANGE:	
APPLICATION NO: <b>Z-165-25-8</b>	DATE: <b>11/20/2025</b>	FROM: <b>GC ( 4.35 ac.)</b>	
GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.  <b>4.35 Acres</b>	3/19/2026	TO: <b>C-2 ( 4.35 ac.)</b>	
	REVISION DATES:		
MULTIPLES PERMITTED GC C-1	AERIAL PHOTO & QUARTER SEC. NO. <b>QS 2-15</b>	ZONING MAP <b>D-5</b>	* UNITS P.R.D OPTION <b>N/A</b> <b>76</b>
	STANDARD OPTION <b>N/A</b> <b>63</b>		

\* Maximum Units Allowed with P.R.D. Bonus



**LAVEEN VILLAGE**  
COUNCIL DISTRICT: 8



APPLICANT'S NAME: <b>Withey Morris Baugh, PLC</b>		REQUESTED CHANGE:	
APPLICATION NO: <b>Z-165-25-8</b>	DATE: <b>11/20/2025</b>	FROM: <b>GC ( 4.35 ac.)</b>	
GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.  <b>4.35 Acres</b>	3/19/2026	TO: <b>C-2 ( 4.35 ac.)</b>	
	REVISION DATES:		
	AERIAL PHOTO & QUARTER SEC. NO. <b>QS 2-15</b>	ZONING MAP <b>D-5</b>	
MULTIPLES PERMITTED	STANDARD OPTION		* UNITS P.R.D OPTION
<b>GC</b>	<b>N/A</b>		<b>N/A</b>
<b>C-1</b>	<b>63</b>		<b>76</b>

\* Maximum Units Allowed with P.R.D. Bonus





RESTAURANT WITH OUTDOOR SEATING



GENERAL RETAIL (INLINE SHOPS)

**SWC 55TH AVE AND SOUTHERN | CHARACTER IMAGES**

03/17/2026

Planning & Development  
Department

MAR 19 2026

CITY OF PHOENIX

March 04, 2026

Dear Laveen Village Planning Committee,

On behalf of Laveen Citizens for Responsible Development (LCRD), we write in support of the rezoning applications for the properties located at the corners of Southern Avenue and 59th Avenue (Z-157-25-8) and Southern Avenue and 55th Avenue (Z-165-25-8).

These parcels are located on a rapidly developing arterial corridor and naturally lend themselves to neighborhood-serving commercial development. The proposed commercial rezoning is compatible with and complementary to the new Cottonfields golf course redevelopment. Thoughtfully planned commercial uses at these intersections can help support nearby residential neighborhoods and reinforce the long-term viability and success of the Cottonfields project.

We appreciate the applicant's proactive engagement with our organization and the surrounding community. It is apparent to us that the applicant has worked collaboratively with the Cottonfields community throughout this process. We appreciate their willingness to address concerns and limit certain undesirable uses, which improves the compatibility of the proposal with the surrounding area.

Although we are in support of the project thus far, we're looking forward to viewing and approving the proposed building designs, landscape plan, lighting plan and sign design.

Sincerely,  
Laveen Citizens for Responsible Development (LCRD)

Sandy Hamilton, Coordinator

**CITY OF PHOENIX**

**MAR 06 2026**

**Planning & Development  
Department**

## Nayeli Sanchez Luna

---

**From:** Patrice Herring <dpwill2@cox.net>  
**Sent:** Thursday, April 2, 2026 5:16 AM  
**To:** Nayeli Sanchez Luna; doc018@yahoo.com; yolie1968@gmail.com  
**Subject:** Laveen 55th and 59th Ave/Southern Rezoning Application Z-157-25-8

**CAUTION: This email originated outside of the City of Phoenix.**

Do not click links or open attachments unless you know the sender and were expecting this email.

[Report Suspicious](#)

Hello Laveen Planning,

I am a resident and owner of property in the Cottonfields Golf Course community adjacent to the parcels on 55th and 59th avenue and Southern in Laveen. We have been lied to about the use of the these parcels by the attorney Withey, Morris, and Baugh and their associates. We were told these parcels would strictly be used for residential housing and the remaining parcels would continue to be golf course. I am greatly opposed to any commercial business in my backyard. Our family bought our home in a golf course community and we have a REMA contract in place which is attached to the parcels to protect us. Any changes have to be approved by the owners of the golf course community attached to the parcels. I hope the city will stop the lying and deceit that continues to occur with Withey, Morris and Baugh law firm in Laveen. This change will negatively affect our community and the safety of the residents who live close to the parcels. Please Do Not approve this zoning change.

Thank you,

Daphne Herring