



City of Phoenix
 PLANNING AND DEVELOPMENT DEPARTMENT

Staff Report Z-40-19-7
August 30, 2019

South Mountain [Village Planning Committee](#) Meeting Date: September 10, 2019

[Planning Commission](#) Hearing Date: October 3, 2019

Request From: [C-2 SPVTABDO](#) (Intermediate Commercial, South Phoenix Village and Target Area B Design Overlay) (1.00 acre)

Request To: [R-4 SPVTABDO](#) (Multifamily Residence District, South Phoenix Village and Target Area B Design Overlay) (1.00 acre)

Proposed Use: Multifamily residential

Location: Approximately 360 feet east of the northeast corner of 2nd Street and Roeser Road

Owner: MVP Apartments II, LLC

Applicant and Representative: Keith Deutscher, Stewart + Reindersma Architecture

Staff Recommendation: Approval, subject to stipulations

General Plan Conformity			
General Plan Land Use Map Designation		Residential 10 to 15 dwelling units per acre	
Street Map Classification	Roeser Road	Minor Collector	33-foot north half street
<p><i>CELEBRATE OUR DIVERSE COMMUNITY; CLEAN NEIGHBORHOODS; Facilitate the acquisition of vacant, underutilized and blighted parcels for appropriate redevelopment, compatible with the adjacent neighborhood character and adopted area plans.</i></p> <p>The site has been vacant and unimproved since 2007. As stipulated, the development is compatible with the adjacent multifamily land uses and appropriate considering the proximity to the proposed light rail station at Central Avenue and Roeser Road.</p>			

CONNECT PEOPLE AND PLACES; TRANSIT ORIENTED DEVELOPMENT; LAND USE PRINCIPLE: Encourage high-density housing and high intensity employment uses to locate adjacent or close to transit stations per adopted transit district plans.

The subject site is located approximately 1,000 feet from a planned light rail station and is located within the planning area for the South Central Light Rail Extension. The site is well situated for walkable urban residential development and will provide additional pedestrian connectivity between the multifamily complex to the north and the proposed station area. The use and scale are consistent with the placetype recommended in the Transit Oriented Development Strategic Policy Framework.

CONNECT PEOPLE AND PLACES; OPPORTUNITY SITES; LAND USE PRINCIPLE: Support reasonable levels of increased intensity, respectful of local conditions and surrounding neighborhoods.

The development, as stipulated, provides a reasonable level of intensity that is respectful to local conditions and surrounding neighborhoods. The multifamily developments to the north and east are under the same ownership as the subject site. The development will provide additional street presence for the larger multifamily complex and pedestrian connectivity to Roeser Road for residents of the area.

Applicable Plans, Overlays, and Initiatives

[South Phoenix Village and Target Area B Design Overlay](#): See Background Item No. 6.

[Tree and Shade Master Plan](#): See Background Item No. 7.

[Transit Oriented Development Strategic Policy Framework](#): See Background Item No. 8.

[Complete Streets Guidelines](#): See Background Item No. 9.

[Reimagine Phoenix](#): See Background Item No. 10.

Surrounding Land Uses/Zoning		
	<u>Land Use</u>	<u>Zoning</u>
On Site	Vacant	C-2 SPVTABDO
North	Multifamily residential	R-4 SPVTABDO
South	Single-family residential	R-4 SPVTABDO
East	Multifamily residential	R-4 SPVTABDO
West	Multifamily residential driveway	R-4 SPVTABDO
	Fire Station (west of driveway)	

R-4 (Multifamily Residential) (Subdivision Development Option)		
<p>The applicant owns property immediately adjacent to the subject site on the east, west, and north. If this zoning request is approved, the applicant will combine the parcels. This will impact required setbacks and development standards; for those impacted, a second standard is provided in the below table.</p>		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the Proposed Site Plan</u>
Gross Acreage	-	1.00 acre
Total Number of Units	Maximum 29	24
Maximum Density	29 du/acre	24 du/acre. (Met)
Maximum Lot Coverage	50%	29.7%. (Met)
Maximum Building height	3 stories or 40 feet	2 stories and 26 feet. (Met)
MINIMUM BUILDING SETBACKS		
Front (Roeser Road)	20 feet	20 feet. Met.
Sides (East and West)	Without lot combination: 10 feet and 3 feet on east and west sides.	West: 21 feet. (Met)
	With lot combination: 10 feet or 3 feet on west side.	East: 35 feet. (Met)
Rear (North)	Without lot combination: 15 feet.	53 feet. (Met)
	With lot combination: 0 feet.	

<i>Perimeter Setbacks / Landscaping</i>		
Minimum Front Setbacks	20 feet	21 feet. (Met)
Minimum Side Setbacks	Without Lot Combination: 5 feet on west. With Lot Combination: 5 feet on west, 0 feet on east	East: 13 feet. (Met) West: 17 feet. (Met)
Minimum Rear Setback	Without Lot Combination: 5 feet; With Lot Combination: 0 feet	0 feet. (Not Met)
Minimum Open Space	5% of gross area	Provided on the larger site. (Not Met)
Minimum Amenities Provided	Minimum 2 amenities	Provided on the larger site. (Not Met)
Parking	1.3 spaces per efficiency; 1.5 spaces per 1 and 2-bedroom units; 2.0 spaces per units with more than 2 bedrooms; 38 spaces required.	35 spaces provided. (Not Met)

Background/Issues/Analysis

SUBJECT SITE

1. The request is to rezone approximately 1.00 acre located approximately 360 feet east of the northeast corner of 2nd Street and Roeser Road from C-2 SPVTABDO (Intermediate Commercial District, South Phoenix Village and Target Area B Design Overlay) to R-4 SPVTABDO (Multifamily Residence District, South Phoenix Village and Target Area B Design Overlay). The applicant is proposing to develop the site as an extension of their multifamily property, zoned R-4 SPVTABDO (Multifamily Residence District, South Phoenix Village and Target Area B Design Overlay), which abuts the site to the north, east, and west. To the west of the site, beyond the multifamily driveway, is a fire station and to the south across Roeser Road is a single-family residential neighborhood.

The site is located approximately 1,000 feet east of the planned light rail station at Central Avenue and Roeser Road. A transit-oriented development policy plan for the area within one-half mile of the proposed rail line is currently in development and will generally encompass the area from 7th Avenue to 7th Street from Jefferson Avenue on the north to South Mountain Avenue on the south.

SURROUNDING LAND USES AND ZONING

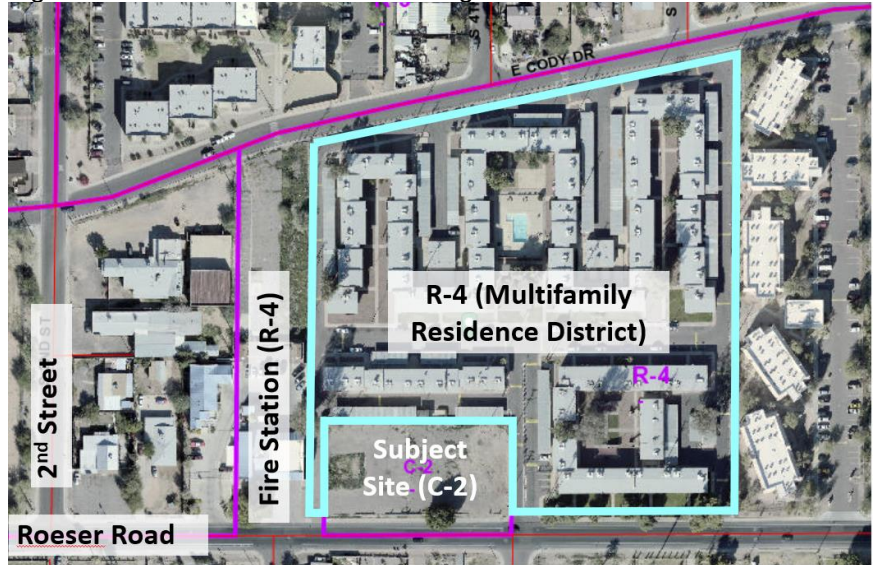
2. The subject site is vacant and has been undeveloped since a structure was demolished on the west half of the site circa 2007. The site is zoned C-2 (Intermediate Commercial).

Immediately to the north and east is an apartment complex zoned R-4 (Multifamily Residence District). To the west is one of the driveways to the apartment complex and the same zoning district wraps around the subject site

To the west, beyond the multifamily driveway, is a fire station zoned R-4 (Multifamily Residence District). The fire station is one-story and approximately 20 feet in height.

To the south of Roeser Road is a single-family neighborhood zoned R-4 (Multifamily Residence District) and all homes are single-story structures. The subject site and surrounding properties are also located within the South Phoenix Village and Target Area B Design Overlay District.

Figure A. Site Context and Surrounding Land Uses



Source: Planning and Development Department

GENERAL PLAN LAND USE MAP DESIGNATIONS

3. The General Plan Land Use Map designation for the subject site is Residential 10 to 15 dwelling units per acre and this designation extends off-site between Roeser Road and Cody Drive from the 1st Street alignment to 7th Street. South of Roeser Road is designated Residential 3.5 to 5 dwelling units per acre. The land use intensity generally increases toward Central Avenue where it is designated Commercial. The site plan proposes 24 dwelling units per acre density which exceeds that depicted by the General Plan Land Use Map; however, because the site is less than 10 acres in size, a General Plan Amendment is not required.

Figure B. General Plan Land Use Map Designation

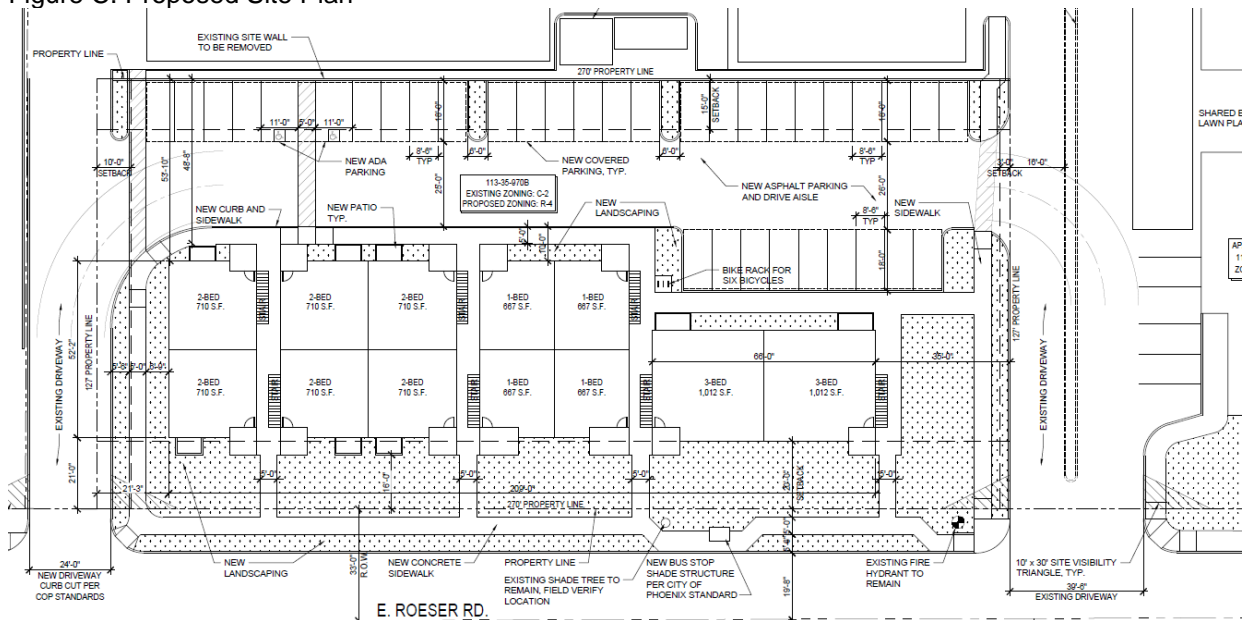


Source: Planning and Development Department

PROPOSAL

- The applicant is proposing 24 dwelling units on a 1.00 acre site. The development is adjacent to a 7.29-acre multifamily development under the same ownership as the subject site. If the request is approved, the applicant intends to complete a lot combination to add the site to the larger complex. The subject site is therefore designed to be an extension of the adjacent multifamily. The parking area is situated behind the building and will be accessed from shared driveways on the east and west sides of the site.

Figure C. Proposed Site Plan



Source: STEWART + REINDERSMA ARCHITECTURE, PLLC

The conceptual site plan calls for a segmented two-story multifamily building. The conceptual elevations include patios, balconies, contrasting trim, and undulating rooflines which run a maximum width (east-west) distance of 62 feet. The architectural enhancements are intended to make the building more approachable and pedestrian-oriented.

Figure D. Proposed Elevations (Left, West Elevation) (Right, East Elevation)

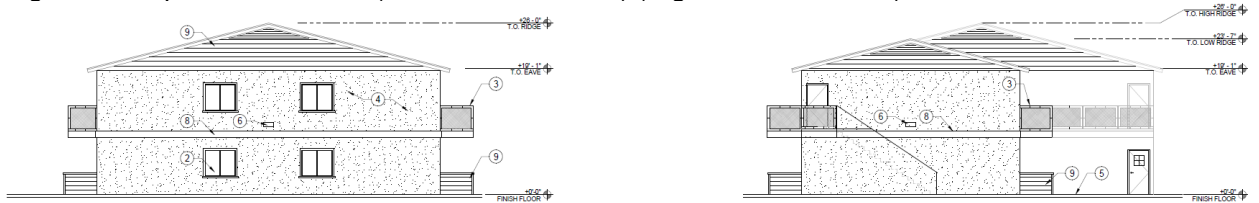


Figure E. Proposed Elevations (Top, Roeser Road Frontage) (Bottom, North Elevation)



Source: STEWART + REINDERSMA ARCHITECTURE, PLLC

5. The applicant intends to combine the parcel with the apartment complex to the north. When combined with the site to the north, residents will utilize the pool and open space amenities in the adjacent apartment complex. The development will include shaded pedestrian connections between the complex to the north and Roeser Road. The development will also utilize existing drive-aisles and curb cuts onto Roeser Road.

If combined with the adjacent development as planned, the site plan would be generally compliant with code; however, as a stand-alone development, the following deficiencies exist:

- A minimum five-foot landscape setback would be required along the north property line. There is no landscape setback proposed.
- A minimum 5 percent common open space is required for all multifamily projects in the City of Phoenix. There are no open spaces provided on the site, however, the larger apartment complex appears to include sufficient open space.

- A minimum of 2 resident amenities are required for all multifamily projects in the City of Phoenix. There are no resident amenities provided on the site, however, the larger apartment complex includes sufficient amenities.

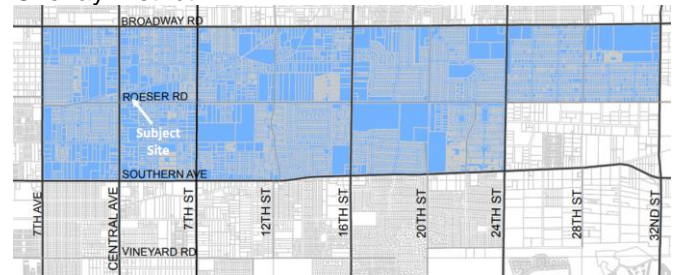
Additionally, the parking supply is deficient by four parking spaces. While the development is situated in close proximity to a proposed light rail station, it is currently ineligible for the 25 percent parking reduction cited in the development standards table.

STUDIES AND POLICIES

6. [South Phoenix Village and Target Area B Design Overlay \(SPVTABDO\)](#)

The purpose of the SPVTABDO is to enhance residential neighborhoods and to provide protection from one and two dwelling units projects per lot residential lots. While the overlay does not apply to the proposed project, it does insist upon quality design. As stipulated, the proposal advances the intent of the overlay by integrating landscaping and covered parking areas.

Figure F. South Phoenix Village Target Area B Overlay District



Source: Planning and Development Department

7. [Transit Oriented Development Strategic Policy Framework](#)

The Transit Oriented Development Strategic Policy Framework is part of the City's General Plan which identified planning typologies to describe urban environments. The subject site is located within a quarter mile of a light rail station area. The identified environment for this light rail station is Neighborhood Center. A Neighborhood Center is a placetype characterized by relatively low intensity with building heights typically from two to four stories with incentive heights up to five stories. Land Uses may include an assortment of medium intensity housing products.

Figure G: Transit Oriented Development Strategic Policy Framework Place Types

STATION LOCATION	PLACE TYPE	PLACE TYPE ICON
Roeser Road	Neighborhood Center	
Southern Avenue	Neighborhood Center	
Baseline Road	Neighborhood Center	



Source: Reinvent Phoenix, Planning and Development Department

Staff is recommending stipulations to incorporate design elements into the proposal including the following that are appropriate due to the proximity to the future light rail station:

- Architectural enhancements to create visual interest and break-up the mass of the building. (See Stipulation No. 1.)
- Accentuated unit entrances to Roeser Road using either a patio or porch treatment as shown on Figure H. (See Stipulation No. 2.)
- Balconies for all upper floor units to provide additional “eyes on the street.” (See Stipulation No. 5.)
- Bicycle racks provided at a transit-oriented standard and located for convenient access. (See Stipulation No. 6.)

The stipulations related to detached and shaded pedestrian thoroughfares described under the heading of the Tree and Shade Master Plan are also strongly aligned with Transit Oriented Development Strategic Policy Framework. These are addressed in Stipulation Nos. 3, 4, And 7.

8. [Tree and Shade Master Plan](#)

The Tree and Shade Master Plan encourages treating the urban forest as infrastructure to ensure the trees are an integral part of the City’s planning and development process. Sidewalks on the street frontages should be detached from the curbs to allow trees to be planted on both sides of the sidewalk to provide thermal comfort for pedestrians and to reduce the urban heat island effect. Staff is recommending several stipulations designed to provide trees and enhance shade within the development.

Staff is recommending a stipulation to require detached sidewalks along Roeser Road with 3-inch caliper trees placed 20-feet on center with an exception to allow for the preservation of a mature shade tree located near a transit stop. This is addressed in

Figure H. Porch Frontage Type



Source: Phoenix Zoning Ordinance, Walkable Urban Code, Frontage Standards

Figure I. Patio Frontage Type



Source: Phoenix Zoning Ordinance, Section 1305 (Walkable Urban Code), Frontage Standards

Stipulation No. 3.

Staff is recommending a stipulation to require detached pedestrian pathways on both the east and west sides of the site from Roeser Road extending to the north property line. These pathways will be planted with 3-inch caliper trees, 20 feet on center, to provide shaded pedestrian paths through the site and connecting the apartment complex to the north, to the sidewalk on Roeser Road. This is addressed in Stipulation No. 4.

Staff is recommending a stipulation to require additional landscaping of the surface parking area to increase thermal comfort on the site and to provide additional landscape buffers between apartment buildings. This is addressed in Stipulation No. 7

9. [Complete Streets Guidelines](#)

In 2014, the City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles.

To support these principles, staff is recommending stipulations to require that all sidewalks be detached with a minimum five-foot wide landscaped strip located between the sidewalk and back of curb and shall include minimum three-inch caliper shade trees planted a minimum of 20 feet on center or equivalent groupings. This is addresses in Stipulation No. 3.

To further advance the goals of the Complete Streets Guidelines, the development, as stipulated, will provide additional high-quality pedestrian access from the apartment complex north of the site to Roeser Road. This is addresses in Stipulation No. 4.

10. [Reimagine Phoenix](#)

As part of the Reimagine Phoenix Initiative, the City of Phoenix is committed to increasing the waste diversion rate to 40 percent by 2020 and to better manage its solid waste resources. The provision of recycling containers was not addressed in the applicant's submittals.

COMMUNITY INPUT SUMMARY

11. As of the writing of this report, staff has not received any correspondence regarding this proposed rezoning application.

INTERDEPARTMENTAL COMMENTS

12. The City of Phoenix Aviation Department commented that the site is within Phoenix Sky Harbor International Airport traffic pattern airspace and the proposed use is multifamily residential. Therefore, the Aviation Department has provided a stipulation requiring the developer to record a Notice to Prospective Purchasers of the proximity to the airport to disclose the operational characteristics of the airport. This is addressed in Stipulation No. 8.
13. The site is located in a larger area identified as being archaeologically sensitive. If further review by the City of Phoenix Archaeology Office determines the site and immediate area to be archaeologically sensitive, and if no previous archaeological projects have been conducted within this project area, it is recommended that archaeological Phase I data testing of this area be conducted. Phase II archaeological data recovery excavations may be necessary based upon the results of the testing. A qualified archaeologist must make this determination in consultation with the City of Phoenix Archaeologist. In the event archaeological materials are encountered during construction, all ground disturbing activities must cease within a 33-foot radius of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. This is addressed in Stipulation Nos. 9, 10, and 11.
14. The Fire Department indicated there are no problems anticipated with the case and that the site and/or buildings shall comply with the Phoenix Fire Code. Further, the Department commented that they do not know the water supply at this site and noted that additional water supply may be required to meet the required fire flow per the Phoenix Fire Code.
15. The Street Transportation Department's Pedestrian Safety Coordinator commented that sidewalks should be detached from the curb along Roeser Road and planted on both sides with single-trunk trees. This is addressed in Stipulation No. 3. Further, the Coordinator noted the desire for traffic calming on site to increase pedestrian comfort and safety.

Findings

1. The request is not consistent with the General Plan Land Use Map designation of Residential 10 to 15 dwelling units per acre; however, a General Plan Amendment is not required because the subject site is less than 10 acres in size. The site is generally consistent with the scale of surrounding land uses.
2. The development, as stipulated, is appropriate at this location due to its compatibility with the surrounding land uses and proximity to a future high-capacity transit line.

3. As stipulated with specific regard to the architectural and pedestrian enhancements, the development is generally consistent with policies and the intent of the City's Transit Oriented Development Strategic Policy Framework.

Stipulations

1. All building facades shall contain architectural embellishments and detailing such as textural changes, pilasters, offsets, recesses, window fenestration, shadow boxes and/or overhead canopies at least every 50 linear feet, as approved by the Planning and Development Department.
2. All ground level units on the south half of the building shall have their individual main entrances oriented to Roeser Road, including a direct pedestrian pathway to the public sidewalk and a porch or patio to accentuate the unit entrance as described below and as approved by the Planning and Development Department:
 - a. Patio Frontage:
 1. Minimum depth of eight feet.
 2. Minimum area of 64 square feet.
 3. Patio shall be enclosed on each side by a low wall not to exceed 40 inches in height.
 4. The area between the property line and main facade of the building which is not improved with permanent hardscape shall be planted with live vegetation ground coverage and shrubs excluding hardscape for pedestrian amenities.
 - b. Porch Frontage:
 1. Minimum depth of six feet.
 2. Minimum width of 50 percent of the facade.
 3. Minimum shade coverage of 75 percent of porch area.
3. All public sidewalks shall be detached with a minimum five-foot-wide landscaped strip located between the sidewalk and back of curb and shall include minimum three-inch caliper shade trees planted a minimum of 20 feet on center or equivalent groupings along both sides of the sidewalk. The sidewalk may be attached in the area of the mature shade tree located approximately 100 feet west of the southeast corner of the subject property, as approved by the Planning and Development Department.
4. The developer shall provide pedestrian pathways on the east and west side of the subject site extending from Roeser Road to the north property line to provide pedestrian access to neighboring multifamily developments. Pedestrian pathways shall include the following, as approved by the Planning and Development Department:

- a. Pedestrian pathways shall be detached from the vehicular areas by a minimum five-foot-wide landscaped area and shall include minimum three-inch caliper shade trees planted a minimum of 20 feet on center or equivalent groupings along both sides of the sidewalk.
 - b. Where pedestrian pathways cross driveways, the crossing shall be delineated by stamped and/or colored crosswalks, as approved by the Planning and Development Department.
5. All individual units above the ground floor shall have a shaded balcony of no less than 50 square feet in total size with a minimum depth of five feet, as approved by the Planning and Development Department.
6. Inverted-U bicycle racks shall be provided at a rate of 0.25 per dwelling unit located near building entries and installed per the requirements of Section 1307.H. of the Phoenix Zoning Ordinance, as approved by the Planning and Development Department.
7. Fifteen percent of the surface parking lot area must be landscaped exclusive of perimeter landscaping. Minimum two-inch caliper shade trees shall be required in the surface parking lot landscape planters, as approved by the Planning and Development Department.
8. The developer shall record a Notice to Prospective Purchasers of Proximity to Airport in order to disclose the existence and operational characteristics of Phoenix Sky Harbor International Airport (PHX) to future owners or tenants of the property.
9. If determined necessary by the Phoenix Archaeology Office, the applicant shall conduct Phase I data testing and submit an archaeological survey report of the development area for review and approval by the City Archaeologist prior to clearing and grubbing, landscape salvage, and/or grading approval.
10. If Phase I data testing is required, and if, upon review of the results from the Phase I data testing, the City Archaeologist, in consultation with a qualified archaeologist, determines such data recovery excavations are necessary, the applicant shall conduct Phase II archaeological data recovery excavations.
11. In the event archaeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33-foot radius of the discovery, notify the City Archaeologist, and allow time for the Archaeology Office to properly assess the materials.

Writer

Nick Klimek
August 30, 2019

Team Leader

Samantha Keating

Exhibits

Zoning sketch map
Aerial sketch map
Conceptual Site Plan date stamped August 7, 2019
Conceptual Elevations date stamped August 7, 2019