



City of Phoenix
PLANNING AND DEVELOPMENT DEPARTMENT

Staff Report Z-8-20-5
June 12, 2020

North Mountain [Village Planning Committee](#) Meeting Date: June 17, 2020

[Planning Commission](#) Hearing Date: August 6, 2020

Request From: C-O (Commercial Office) (3.24 acres)

Request To: R-3A (Multifamily Residence District) (3.24 acres)

Proposed Use: Multifamily Residential

Location: Approximately 220 feet north of the northeast corner of 23rd Avenue and West Royal Palm Road

Owner: Matthew R Luxenberg Family Trust

Applicant: Kris Losch

Representative: Kris Losch

Staff Recommendation: Approval, subject to stipulations

General Plan Conformity			
General Plan Land Use Map Designation		Residential 15+ Dwelling Units Per Acre	
Street Map Classification	23rd Avenue	Minor Collector	30 foot east half street
<p><i>CONNECT PEOPLE AND PLACES CORE VALUE; OPPORTUNITY SITES; LAND USE PRINCIPLE: Promote and encourage compatible development and redevelopment with a mix of housing types in neighborhoods close to employment centers, commercial areas, and where transit or transportation alternatives exist.</i></p> <p>The development, as stipulated, will create new housing opportunities in the area and within close proximity to a light rail station. By converting a low occupancy office building to multifamily residential, the project will add additional residents to an area with access to high capacity transit, employment opportunities, and commercial services.</p>			

CONNECT PEOPLE AND PLACES CORE VALUE; BICYCLES; DESIGN PRINCIPLE: Development should include convenient bicycle parking.

The development, as stipulated, will feature bicycle parking and bicycle facilities as a central component of the project in order to facilitate bicycling as a way of life. Features include secure bicycle parking for residents, artistic style racks for guests, and a public bicycle repair station.

BUILD THE SUSTAINABLE DESERT CITY CORE VALUE; TREE AND SHADE; DESIGN PRINCIPLE: Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.

The development, as stipulated, will create an attractive streetscape with detached and well shaded sidewalks. Additionally, the development, as stipulated, will feature enhanced perimeter landscaping. The shade incorporated into the development along its frontages will reduce the urban heat island effect and increase thermal comfort which will make the area more walkable, bikeable, and sustainable.

Applicable Plans, Overlays, and Initiatives

[Transit Oriented Development Strategic Policy Framework](#): See Background Item Nos. 9 and 10.

[Tree and Shade Master Plan](#): See Background Item No. 11.

[Complete Streets Guidelines](#): See Background Item No. 12.

[Comprehensive Bicycle Master Plan](#): See Background Item 13.

[Reimagine Phoenix](#): See Background Item No. 14.

Surrounding Land Uses and Zoning

	<u>Land Use</u>	<u>Zoning</u>
On Site	Commercial	C-O
North	Vacant Building	R-3A
South	Multifamily community under construction	R-3A
East	Multifamily community under construction	R-3A
West	Office / Commerce Park	IND.PK.

R-3A (Multifamily Residential) (Planned Residential District Development Option)		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the Proposed Site Plan</u>
Gross Acreage	-	3.24 acres
Total Number of Units	-	64 units
Maximum Density (dwelling units per acre)	23.1, 26.4 with bonus	19.75 (Met)
Maximum Lot Coverage	45%	Not depicted
Maximum Building height	3 stories or 40 feet for first 150 feet; 1 foot in 5 foot increase to 48 feet, 4-story maximum	2 stories, approximately 30 feet (Met)
Perimeter Setback (Street)	20 feet	Approximately 177 feet (Met)
Perimeter Setback (Non-Street) (North, South, and East)	15 feet adjacent to property line	Approximately 5 feet (Not Met*)
Perimeter Building Setback (Street) (West)	10 foot minimum	Approximately 177 feet (Met)
Minimum Open Space	5 percent of gross area	Not depicted
Minimum Amenities Provided	Minimum 2 amenities	Not depicted
Parking	128 spaces required.	115 spaces provided (Not Met*)
<i>*Variance or site plan adjustment required</i>		

Background/Issues/Analysis

SUBJECT SITE

1. This request is to rezone 3.24 acres from C-O (Commercial Office) to R-3A (Multifamily Residence District) to allow for the adaptive reuse conversion of an existing office building for multifamily. The site is located approximately one-half mile from the light rail station located at 19th Avenue and Northern Avenue. The site is also located within a neighborhood known as 19North which has been working with the Phoenix Planning and Development Department to develop a transit oriented development policy plan for the last two years.

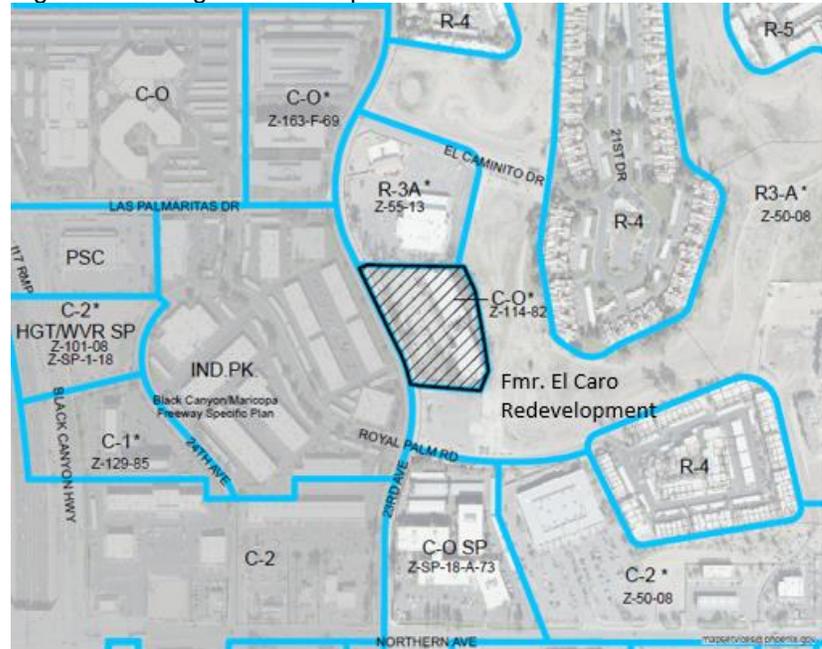
A significant amount of public engagement has occurred to establish a vision for this area. The resident engagement from the 19North policy planning efforts and those through the development of the “Transit Oriented Development Strategic Policy Framework” envisioned this area for walkable urban development and redevelopment.

SURROUNDING LAND USES AND ZONING

2. The proposed multifamily residential zoning is consistent and complementary to the surrounding land uses. To the north is a vacant building with R-3A (Multifamily Residence District) zoning.

To the east and south is a multifamily project currently under development with R-3A (Multifamily Residence District) zoning. To the west, across 23rd Avenue, is a commerce park with IND.PK (Industrial Park) zoning.

Figure A: Zoning Context Map



Source: City of Phoenix Planning and Development Department

The proposed development would reposition an underutilized building in close proximity to high-capacity transit while also completing an emerging zoning pattern for multifamily north of Royal Plan Road and east of 23rd Avenue.

3. In 2009, the former El Caro Golf Course was rezoned through Z-50-08 subject to stipulations to allow for multifamily redevelopment of the property. In 2019, an applicant modified the stipulations of approval through Z-50-08 (PHO-5-19) to enable the redevelopment of the western portion of the former golf course as rental townhomes.

The multifamily development is currently underway and is located immediately adjacent to the subject site, sharing its east property line.

Figure B: Site Plan for adjacent property, under construction



Source: LVA Landscape Architecture

Figure C: Example Conceptual Building Elevations for adjacent property, under construction



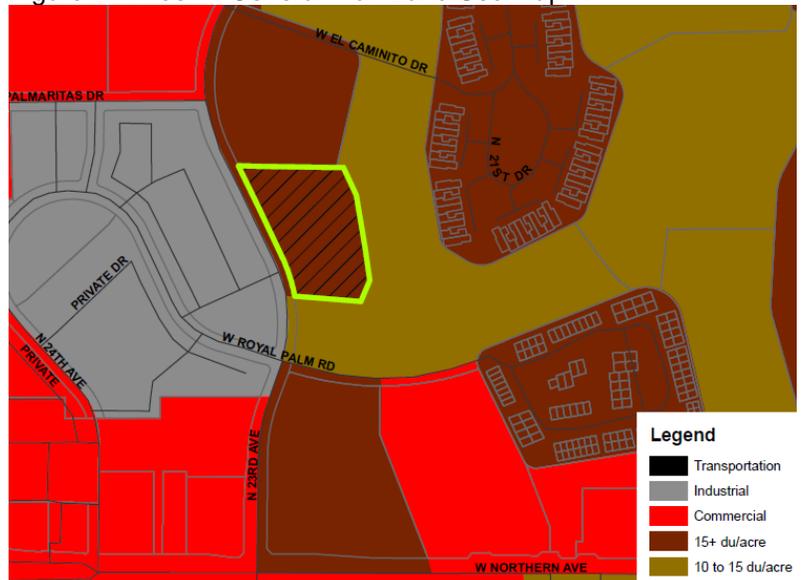
Source: KHovnanian Homes

GENERAL PLAN LAND USE MAP

4. The General Plan Land Use Map designation for the subject site is Residential 15+ Dwelling Units Per Acre. The request for approximately 20 dwelling units per acre is consistent with the designation.

The multifamily project to the east and south is designated as Residential 10 to 15 Dwelling Units per acre. The vacant commercial building to the north is designated as Residential 15+ Dwelling Units Per Acre.

Figure D. Phoenix General Plan Land Use Map



Source: City of Phoenix, Planning and Development Department.

PROPOSAL

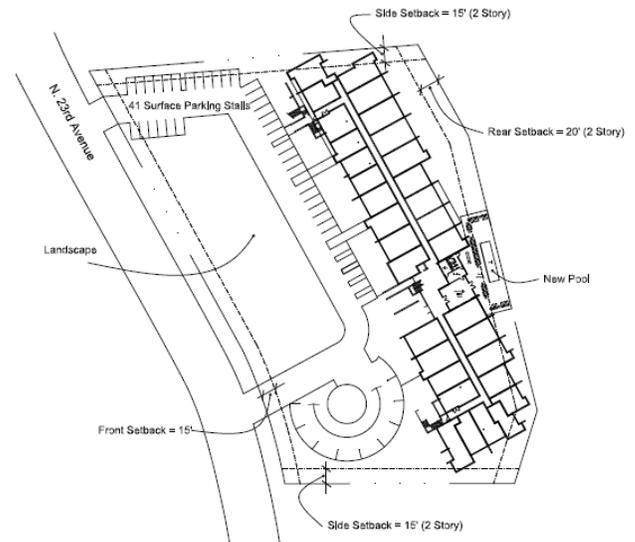
5. Site Plan

The site currently has two points of ingress / egress from 23rd Avenue, an office building of approximately 63,000 square feet, a surface parking lot, and an underground parking garage between the street and the building.

The site plan depicts the conversion of the building for 64 units comprised of 1 and 2-bedroom units. A share of the surface parking between the building and the street will be converted to landscape area.

The site plan depicts a pool amenity and a substantial amount of open space and staff is recommending that 10 percent of the site is retained as open space. This is addressed in Stipulation No. 10.

Figure E: Conceptual Site Plan



Source: Trillium Residential

Staff is recommending a series of stipulations to facilitate walkable, urban, and transit oriented redevelopment as envisioned and supported by the Transit Oriented Development Strategic Policy Framework and the emerging vision which will be described in the 19North Policy Plan. Please see background item Nos. 9 and 10 for more discussion on the TOD Strategic Policy Framework These stipulations are as follows:

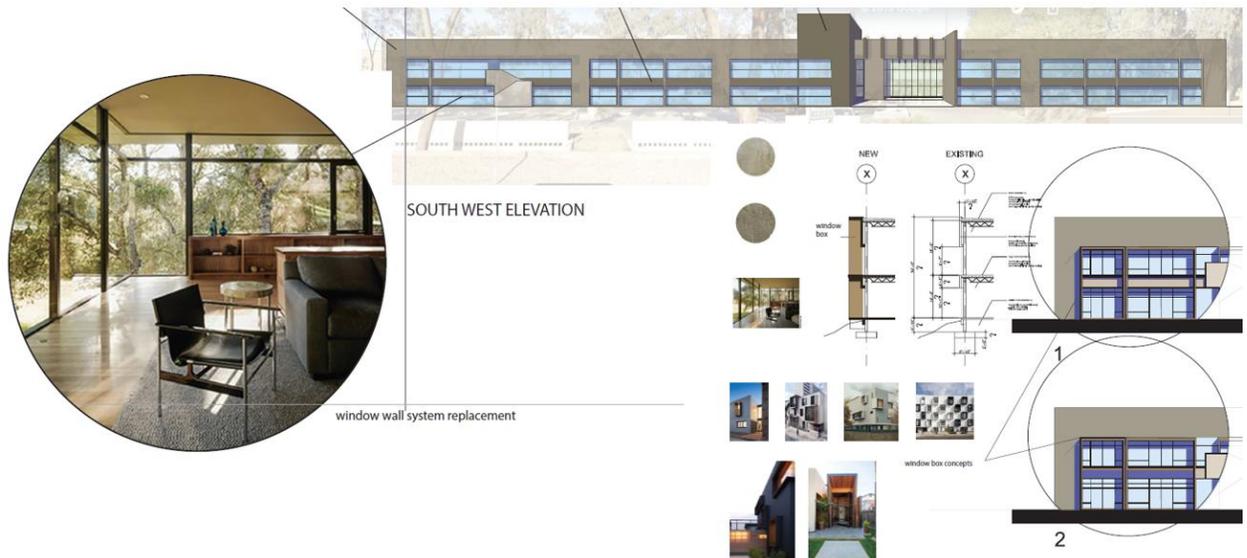
- Stipulation No. 3 requires detached sidewalks, shaded to 75 percent by large canopy shade trees planted between the curb and sidewalk. To further buffer the sidewalk from vehicular traffic, the stipulation requires low shrubs planted between the curb and the sidewalk.
- Stipulation No. 4 requires an enhanced pedestrian connection between the public sidewalk, the first phase of development, and to any subsequent phases of development.
- Stipulation No. 6 requires that surface parking areas be setback a minimum of 30 feet or screened by buildings.
- Stipulation No. 7 prohibits solid (opaque) perimeter walls over 36 inches in height between the plane of the buildings and 23rd Avenue.

Staff is recommending an enhanced setback and planting standards to promote compatibility between the development and the multifamily community being developed to the east. Stipulation No. 8 requires a minimum average setback between the development and the east property line of 25 feet and is intended to vegetate and existing setback area in a manner to both buffer and improve thermal comfort. This required setback area will be planted with shade trees to an enhanced planting specification.

6. Conceptual Elevations

The applicant provided conceptual building elevations and interior unit concepts. The concepts depict enhancements such as window boxes, a series of paint colors, and window wall replacement systems to provide ample lighting into residential units. Staff is recommending a stipulation to require architectural enhancements on all four sides of the existing building; this is addressed in Stipulation No. 2.

Figure F: Conceptual Front Elevation and Enhancements



Source: Trillium Residential

7. Planning for Subsequent Phases

The site plan does not depict any development beyond the adaptive reuse of the building; however, the applicant provided conceptual floor plans and building elevations for a second phase of single-family attached products that would be located along 23rd Avenue atop the existing parking garage facility.

Figure G: Conceptual Phase II Floor Plans and 23rd Avenue Elevations



Source: Trillium Residential

8. Staff is recommending a series of stipulations in the case that additional units be added to the site in addition to that depicted on the conceptual site plan. The purpose of these stipulations is to engage the 23rd Avenue frontage by making it visible and accessible from units, to promote high-quality and human-scale architecture, and to ensure the overall development functions seamlessly as a single neighborhood. These issues are addressed Stipulation No. 1 with the following subpoints.
- Stipulation No. 1.a. notes that all stipulations pertaining to the first phase of development also apply to any subsequent phases.
 - Stipulation No. 1.b-c. requires unit entrances be oriented to 23rd Avenue and include a porch or patio.
 - Stipulation No. 1.d. requires that all garage doors be screened from the exterior of the site and incorporate decorative embellishments.

STUDIES AND POLICIES

9. **Transit Oriented Development Strategic Policy Framework:**

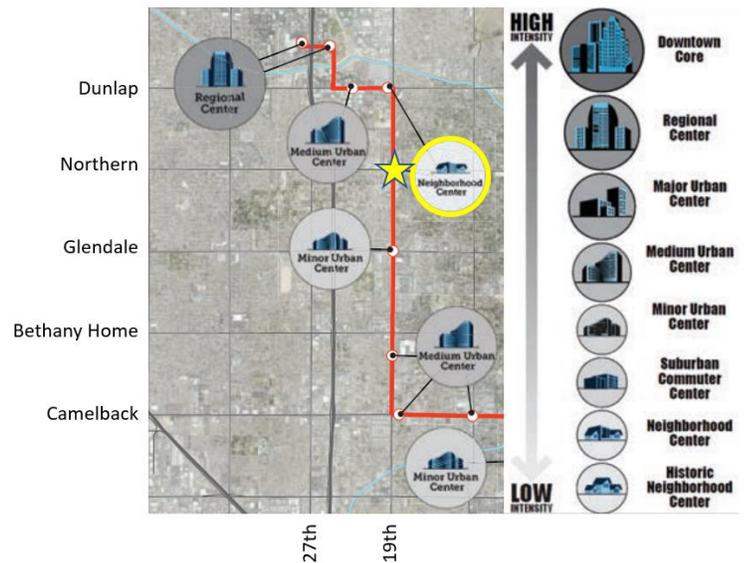
The Transit Oriented Development Strategic Policy Framework is part of the City’s General Plan. The framework identifies planning typologies to describe urban environments. The subject site is located within approximately one-half mile of a future rail station in an area identified as a Neighborhood Center Place Type.

The Neighborhood Center Place Type is characterized by 2 to 4 story buildings and an assortment of housing products including apartments and townhomes in addition to limited retail and employment uses. The proposed use is consistent with the approved Place Type.

10. To promote walkable, urban, and transit oriented development, staff is recommending the following stipulations:

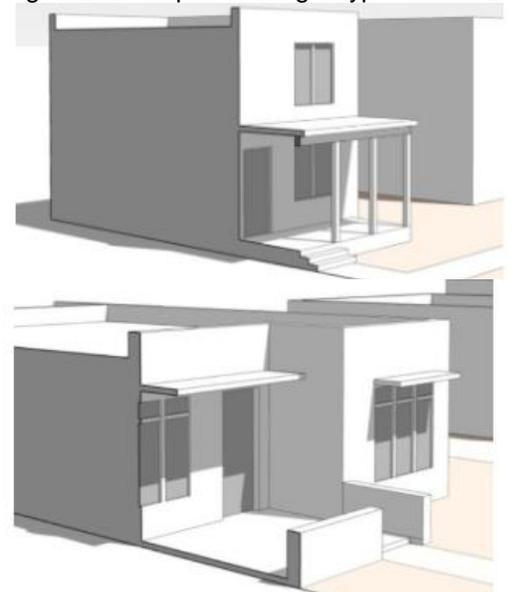
- Stipulation 1.b through c. requires residential units to be oriented to 23rd Avenue with porches or patios connected to the public sidewalk.
- Stipulation No. 3 requires public sidewalks to be detached and shaded to 75 percent.

Figure H: Place Type Map from the Transit Oriented Development Strategic Policy Framework



Source: Planning and Development Department

Figure I. Example Frontage Types



Source: Phoenix Zoning Ordinance

11. **Tree and Shade Master Plan**

The Tree and Shade Master Plan encourages treating the urban forest as infrastructure to ensure the trees are an integral part of the City's planning and development process. Sidewalks on the street frontages should be detached from the curbs to allow trees to be planted on both sides of the sidewalk to provide thermal comfort for pedestrians and to reduce the urban heat island effect.

Staff is recommending a series of stipulation to implement the goals and vision of the Tree and Shade Master Plan including the following:

- Stipulation No. 3 requires shaded and detached sidewalks with enhanced tree planting standards.
- Stipulation No. 6 requires enhanced vegetative coverage over all surface parking areas required. While shade would be ideal, much of the surface parking is located on top of a parking structure which may compromise the ability to maintain a healthy tree canopy; as an alternative, the stipulation focuses on vegetative ground cover to mitigate the urban heat island effect.
- Stipulation No. 8 requires more and larger trees in the eastern setback area to serve as a buffer with the neighboring property and as shade for the required amenity area.

12. **Complete Streets Guidelines**

In 2014, the City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles.

Staff is recommending stipulations for comfortable, shaded, and enhanced pedestrian pathways to and from the site (Stipulation Nos. 3 and 4) and for enhanced bicycle infrastructure including parking and a bicycle repair station (Stipulation No. 9). The development, as stipulated, creates a residential environment designed to allow its residents to use alternative transportation such as transit, walking, or riding a bike to destinations such as a transit station or regional park.

13. **Comprehensive Bicycle Master Plan**

The Comprehensive Bicycle Master Plan is a guide for creating a culture and environment conducive to bicycling as a viable mode of transportation. The proximity to high capacity transit underscores the importance of walking and bicycling in this area.

Staff is recommending a stipulation to require bicycle parking and a bicycle repair station (Stipulation No. 9). The development, as stipulated, will contribute to a culture of

bicycling, increase multi-modal trips including a bicycle, and provide convenient access to nearby recreation areas.

A bicycle repair station (fix-it station) is vertical infrastructure which contains the necessary tools to make light to moderate repairs to bicycles. The station includes a repair stand and pump affixed to the ground, and standard repair tools connected to the station. Figure J depicts an example of a repair station.

Figure J: Example of a Fix-It Station.



Source: Dero

14. [Reimagine Phoenix](#)

As part of the Reimagine Phoenix Initiative, the City of Phoenix is committed to increasing the waste diversion rate to 40 percent by 2020 and to better manage its solid waste resources. The provision of recycling containers was not addressed in the applicant's submittals.

COMMUNITY CORRESPONDENCE

15. As of the writing of this report, staff has one letter of written correspondence in support of the proposed rezoning application.

INTERDEPARTMENTAL COMMENTS

16. The Fire Department commented that the site plan must comply with the Phoenix Fire Code and further indicated there are no problems anticipated with the case. Further, the Department commented that they do not know the water supply at this site and noted that additional water supply may be required to meet the required fire flow per the Phoenix Fire Code.
17. The Public Works Department, Floodplain Management Division determined the parcel is not in a Special Flood Hazard Area (SFHA), but located in a Shaded Zone X, on panel 1740 L of the Flood Insurance Rate Maps (FIRM) dated October 16, 2013.
18. The Street Transportation Department provided Stipulation No. 11 requiring that the

developer construct all street improvements associated with the development and that all improvements comply with ADA Accessibility Standards.

The Street Transportation Department's Office of Pedestrian Safety commented regarding the public sidewalk and associated improvements including the below:

- Detached sidewalks, shade trees, and low shrubs. These items are addressed in Stipulation No. 3.
- Delineating on-site and off-site pedestrian pathways and reducing vehicular conflicts. These items are addressed in Stipulation Nos. 1, 3, and 4.
- Restrict opaque perimeter walls between the development and 23rd Avenue. This is addressed in Stipulation No. 7.
- Providing secure bicycle parking. This is addressed in Stipulation No. 9.

OTHER

19. The site has not been identified as being archaeologically sensitive. However, in the event archaeological materials are encountered during construction, all ground disturbing activities must cease within 33-feet of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. This is addressed in Stipulation No. 12.
20. Development and use of the site is subject to all applicable codes and ordinances. Zoning approval does not negate other ordinance requirements. Other formal actions such as, but not limited to, zoning adjustments and abandonments may be required.

Findings

1. The development, as stipulated, advances the purpose and intent of several core values from the Phoenix General Plan including policy documents such as the Transit Oriented Development Strategic Policy Framework, the Tree and Shade Master Plan, Complete Streets Guidelines, and the Comprehensive Bicycle Master Plan.
2. The development, as stipulated, is appropriate at this location due to its compatibility with the surrounding land uses and proximity to a future high-capacity transit line.
3. The proposed development is consistent with the Neighborhood Center Place Type from the City's Transit Oriented Development Strategic Policy Framework.

Stipulations

1. For any subsequent phases of development beyond the adaptive reuse of the building depicted on the site plan date stamped March 27, 2020, the following stipulations shall apply, as approved or modified by the Planning and Development Department:
 - a. The stipulations contained in Stipulation Nos. 2 through 15 shall also apply to any subsequent phases of development.
 - b. All units adjacent to 23rd Avenue shall have an individual pedestrian entrance oriented to the public street and be connected to the public sidewalk by a direct pedestrian pathway.
 - c. All units adjacent to 23rd Avenue shall have a porch or patio, described as follows and as approved by the Planning and Development Department:
 - i. Porches shall have a minimum dimension of six feet in depth by a width of no less than 50 percent of the total unit; be oriented to the public right-of-way; have a unit entrance; and have a direct pedestrian connection to the public sidewalk.
 - ii. Patios shall have a minimum dimension of eight feet in depth by a width and a minimum size of 64 square feet; be enclosed on each side by a low wall or fence not to exceed 36 inches in height; be oriented to the public right-of-way; have a unit entrance; and have a direct pedestrian connection to the public sidewalk.
 - d. All garage doors shall be screened by buildings from the exterior of the site and have decorative embellishments such as window panels, color and added materials for the pillars surrounding the door.
2. All elevations of the buildings shall contain architectural embellishments and detailing, such as textural changes, pilasters, offsets, recesses, variation in window size and location, and/or overhang canopies.
3. The public sidewalk along 23rd Avenue shall be detached with a minimum 5-foot-wide landscape area located between the sidewalk and back of curb and planted to the following standards, as approved by the Planning and Development Department. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.

- a. Large canopy, single-trunk shade trees shall be placed 25 feet on center or in equivalent groupings. Twenty-five percent of the required trees shall be a minimum 4-inch caliper and 75 percent shall be a minimum 3-inch caliper.
 - b. At tree maturity, the trees shall shade the sidewalk to a minimum 75 percent
 - c. Drought tolerant shrubs and vegetative groundcovers with a maximum mature height of 24 inches to provide a minimum of 75 percent live coverage at maturity.
4. The developer shall provide a pedestrian thoroughfare between the public sidewalk, and the required amenities contained in the first phase, as described below and as approved by the Planning and Development Department.
 - i. A bollard light path or lighted art feature to accentuate the entrance of the pedestrian thoroughfare from the intersection with the public sidewalk.
 - ii. The pedestrian thoroughfare shall be shaded to a minimum 75 percent by shade trees, vegetative trellis, and/or architectural shade. Trees shall be a minimum 2-inch caliper.
 - iii. A minimum 5-foot-wide landscape area shall be provided along both sides of the pedestrian thoroughfare. In areas where the pedestrian thoroughfare passes between buildings, the landscape area may be reduced to 3 feet in width (or 6 feet on one side only)
 - iv. A maximum of two drive aisle crossings shall be permitted and where crossings occur, the pathway shall be constructed to visually contrasts with the parking and drive aisle surfaces.
5. Surface parking shall be setback a minimum of 30-feet or screened 100 percent by buildings from 23rd Avenue, as approved by the Planning and Development Department.
6. Surface parking areas shall include the below, as approved by the Planning and Development Department:
 - a. A minimum 10 percent of the total surface parking and vehicular circulation area shall be vegetated.
 - b. Landscape planters located at the end of each row of parking and

approximately every 100 linear feet.

- c. Landscape planters shall be a minimum width of eight feet and a minimum 120 square feet for a single row of parking, and a minimum 240 square feet for a double row of parking.
 - d. Landscape planters shall be planted with minimum five-gallon drought tolerant shrubs to achieve 75 percent ground cover. If trees are to be included, they shall be minimum 2-inch caliper shade trees
7. No solid perimeter wall greater than 36 inches in height, situated between the plane of the buildings and the 23rd Avenue right of way, shall be permitted.
8. There shall be an average landscape setback of 25 feet measured from the east property line with the minimum setback being 5 feet. The landscape setback shall be planted with shade trees placed 20 feet on center or in equivalent groupings with 25 percent of the trees being a minimum 4-inch caliper and 75 percent being a minimum 3-inch caliper, as approved by the Planning and Development Department.
9. The developer shall incorporate bicycle infrastructure as described below and as approved by the Planning and Development Department.
 - a. A bicycle repair station (“fix it station”) shall be provided and be maintained near the southern-most vehicular access in an area of high visibility, convenient pedestrian access, and available from the public sidewalk.
 - b. “Secure/Covered Facilities” and/or “Outdoor/Covered Facilities” shall be provided for residents at a rate of 0.25 spaces per dwelling unit, up to a maximum of 50 spaces. Appropriate facilities are defined in Appendix K or the Comprehensive Bicycle Master Plan.
 - c. Artistic style bicycle racks with capacity for a minimum of six bicycles for guests shall be located near the primary entrance to the building depicted on the site plan date stamped March 27, 2020. The artistic racks shall adhere to the City of Phoenix Preferred Designs in Appendix K or the Comprehensive Bicycle Master Plan, as approved by the Planning and Development Department.
10. A minimum of 10 percent of the gross project shall be retained as open space, exclusive of required landscape setbacks, which shall be shaded to a minimum 50 percent by minimum 2-inch caliper large canopy shade trees, as approved by the Planning and Development Department.

11. The developer shall construct all streets within and adjacent to the development with paving, curb, gutter, sidewalk, curb ramps, streetlights, median islands, landscaping and other incidentals, as per plans approved by the Planning and Development Department. All improvements shall comply with all ADA accessibility standards.
12. In the event archaeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33- foot radius of the discovery, notify the City Archaeologist, and allow time for the Archaeology Office to properly assess the materials.

Writer

Nick Klimek

June 12, 2020

Team Leader

Samantha Keating

Exhibits

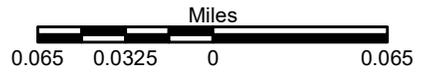
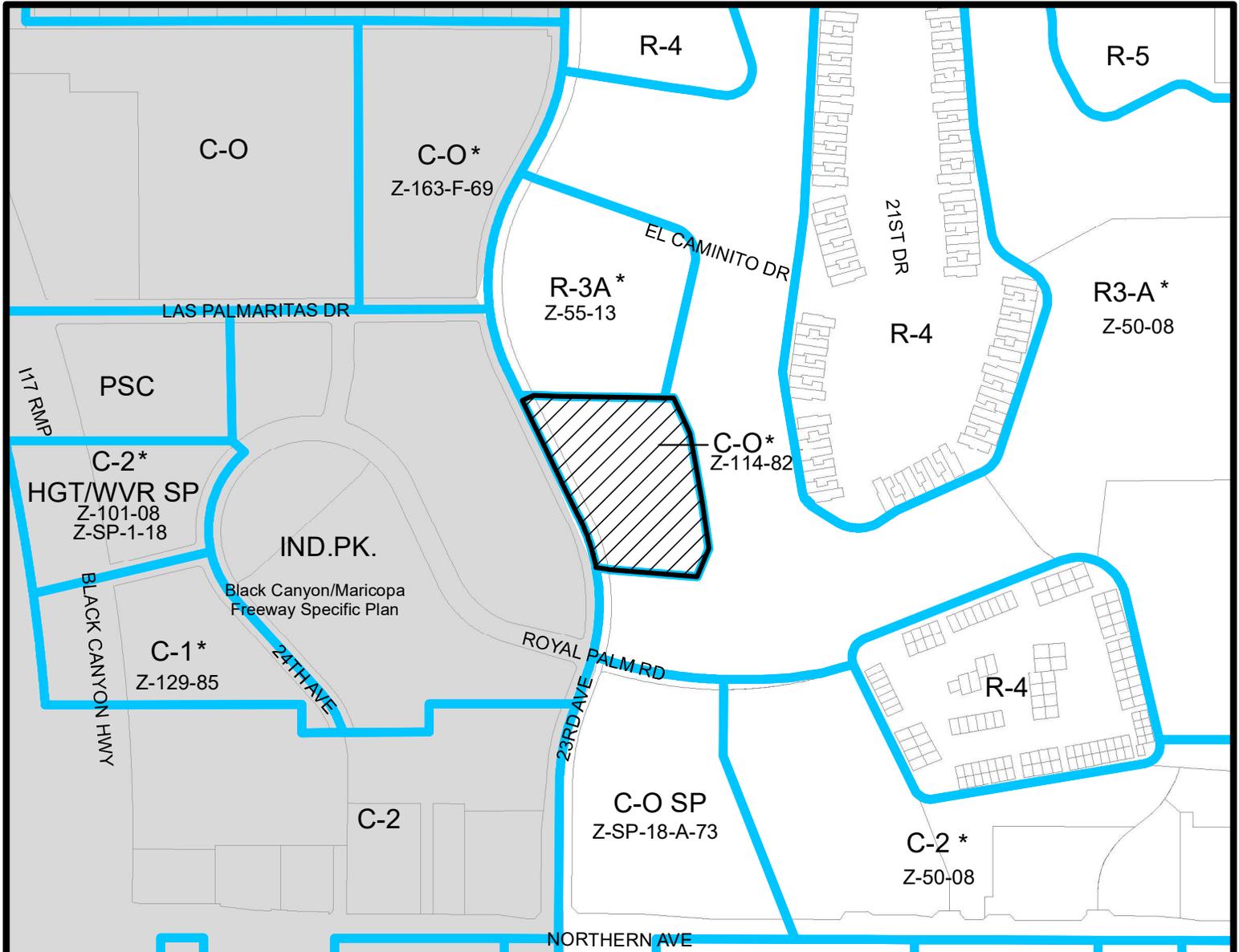
Zoning sketch map

Aerial sketch map

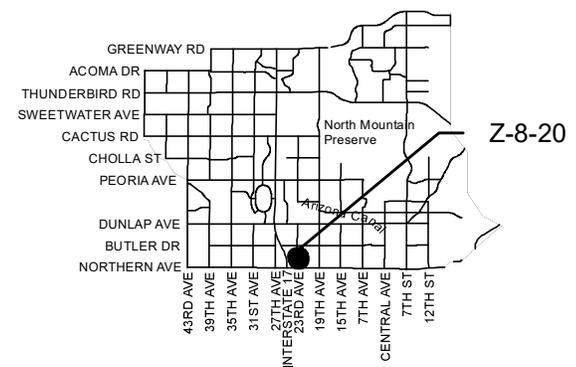
Conceptual Site Plan date stamped March 27, 2020

Conceptual Elevations date stamped March 27, 2020 (4 pages)

Community Correspondence (3 pages)

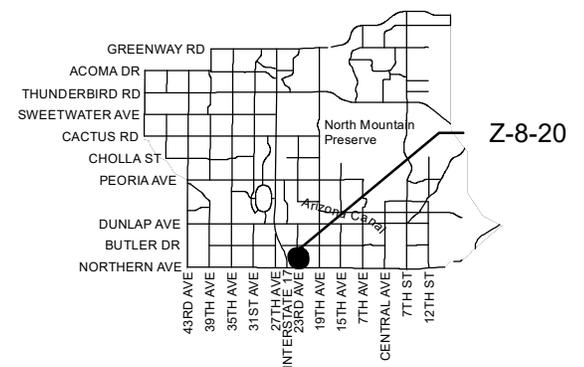
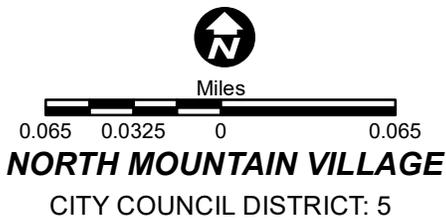
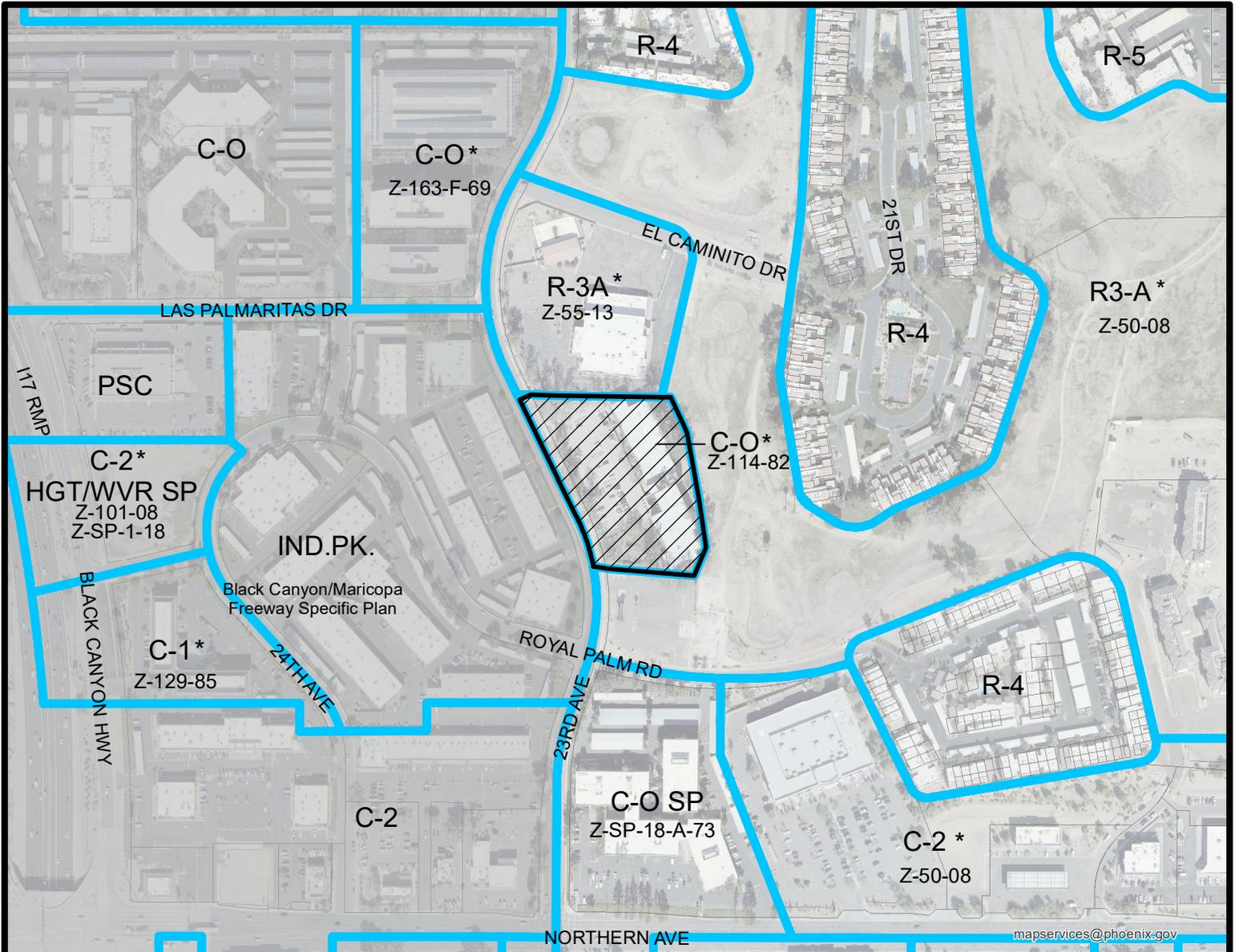


NORTH MOUNTAIN VILLAGE
CITY COUNCIL DISTRICT: 5



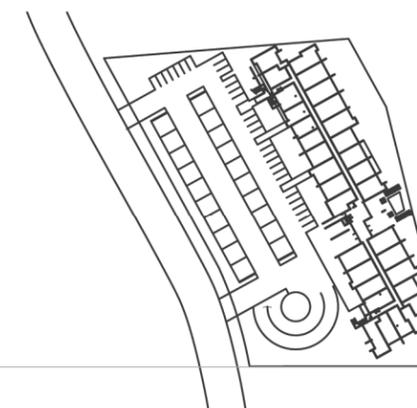
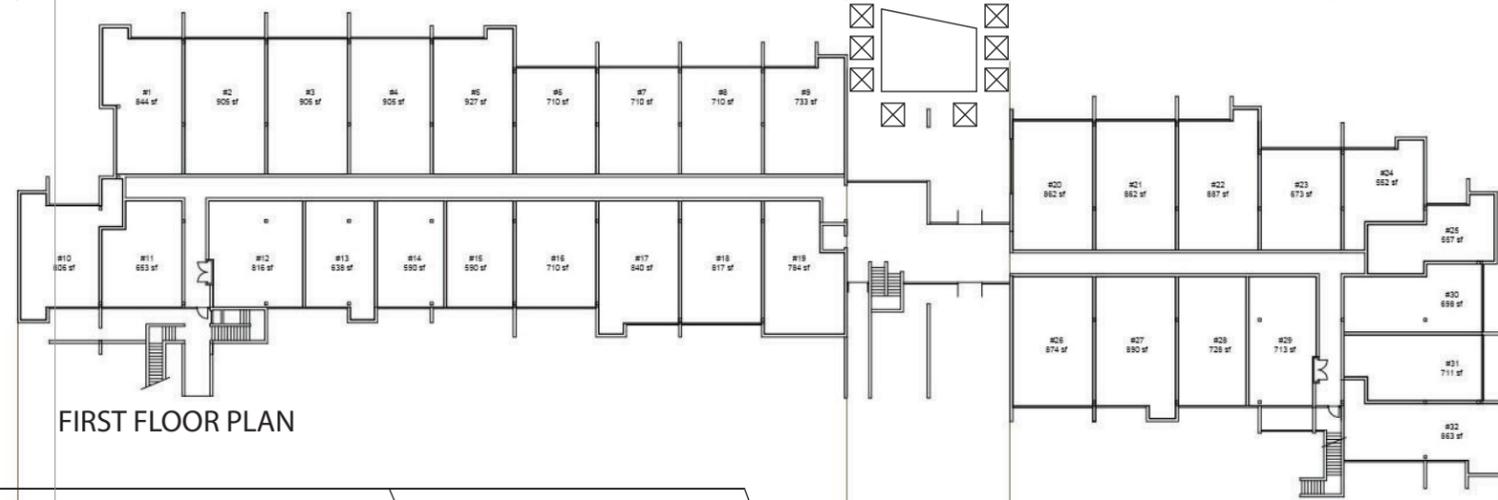
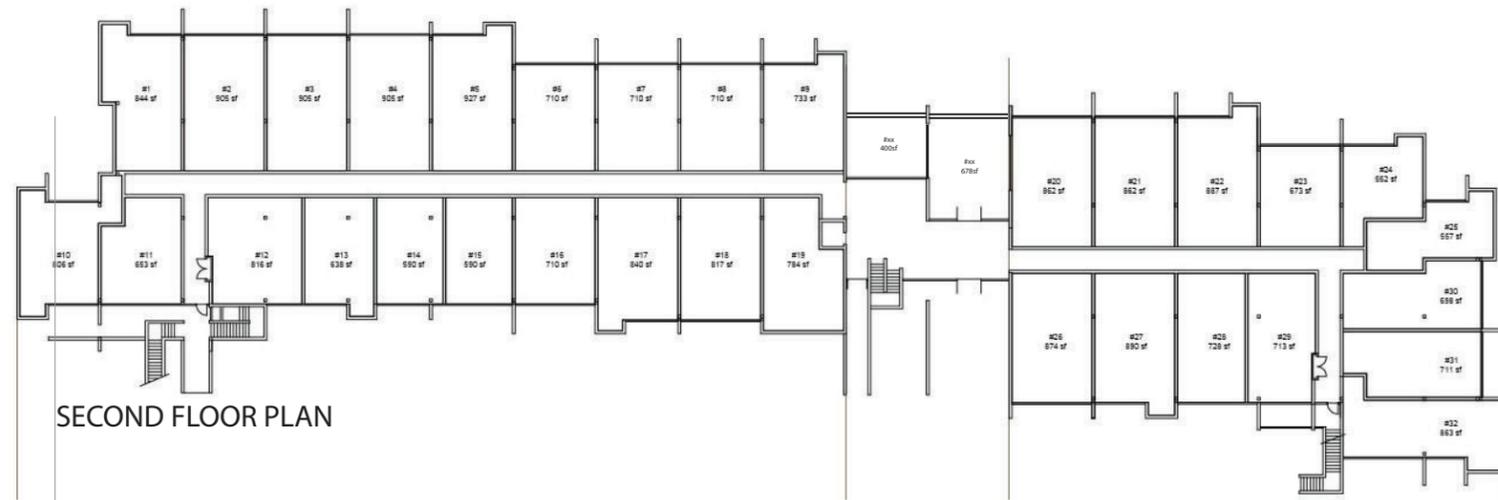
APPLICANT'S NAME: Kris Losch		REQUESTED CHANGE:	
APPLICATION NO. Z-8-20		FROM: C-O (3.24 a.c.)	
DATE: 4/06/2020 <small>REVISION DATES:</small>		TO: R-3A (3.24 a.c.)	
<small>GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.</small> 3.24		<small>AERIAL PHOTO & QUARTER SEC. NO.</small> QS 25-23	
<small>MULTIPLES PERMITTED</small>		<small>CONVENTIONAL OPTION</small>	
C-O		N/A	
R-3A		71	
<small>* UNITS P.R.D. OPTION</small>		<small>* UNITS P.R.D. OPTION</small>	
C-O		N/A	
R-3A		85	

* Maximum Units Allowed with P.R.D. Bonus



APPLICANT'S NAME: Kris Losch		REQUESTED CHANGE:	
APPLICATION NO. Z-8-20		FROM: C-O (3.24 a.c.)	
DATE: 4/06/2020 <small>REVISION DATES:</small>		TO: R-3A (3.24 a.c.)	
<small>GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.</small> 3.24			
<small>AERIAL PHOTO & QUARTER SEC. NO.</small> QS 25-23		<small>ZONING MAP</small> J-7	
MULTIPLES PERMITTED		* UNITS P.R.D. OPTION	
C-O		N/A	
R-3A		85	

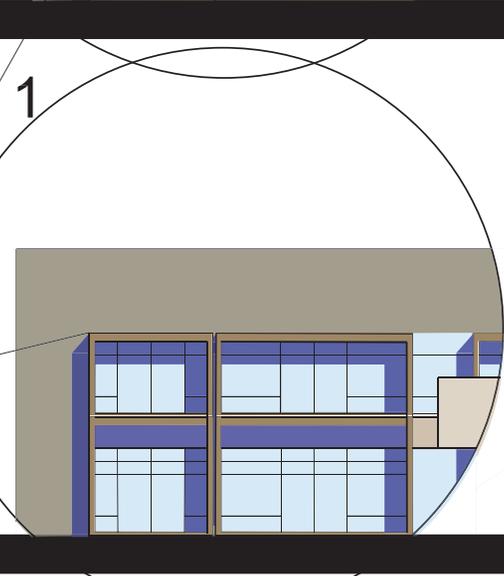
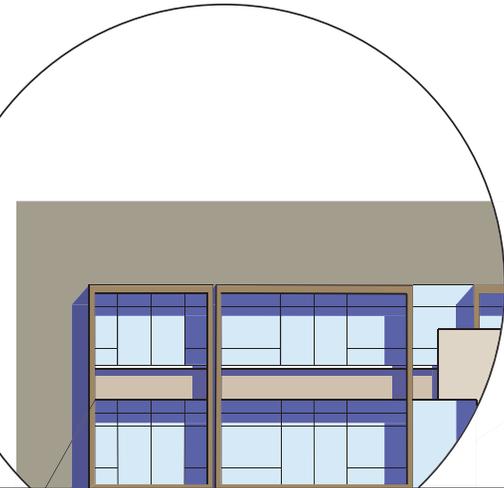
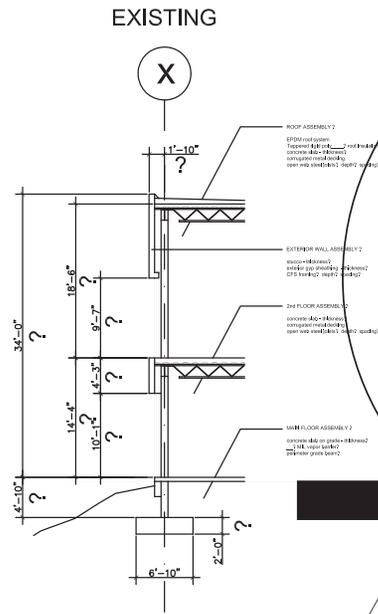
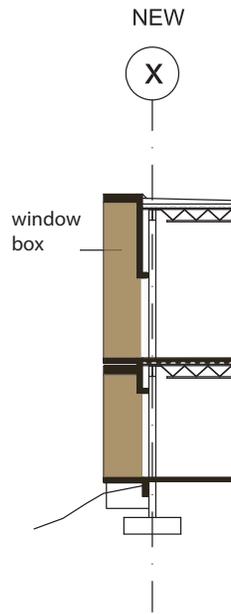
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CITY OF PHOENIX

MAR 27 2020

Planning & Development Department

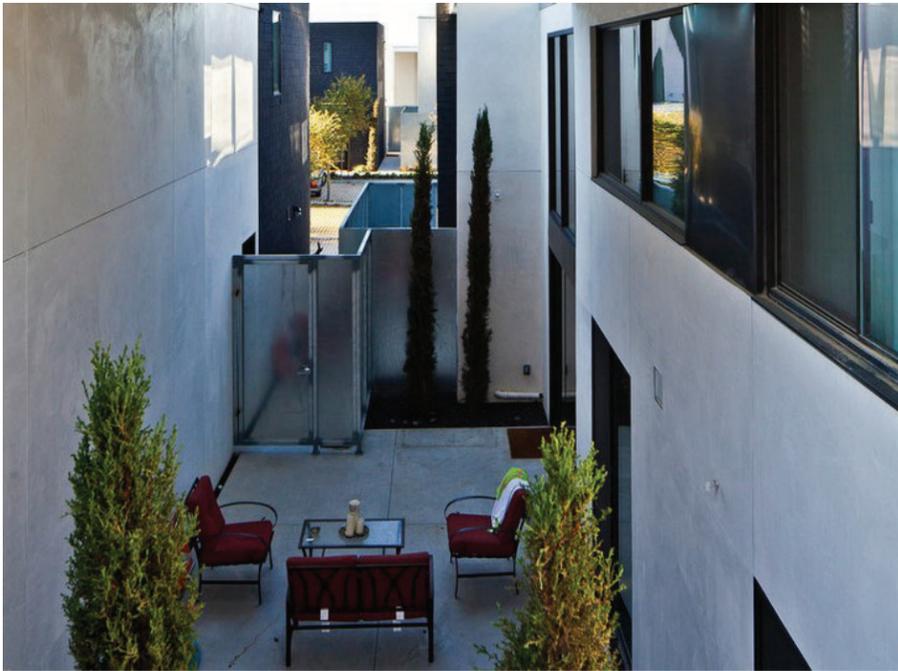


window box concepts



1

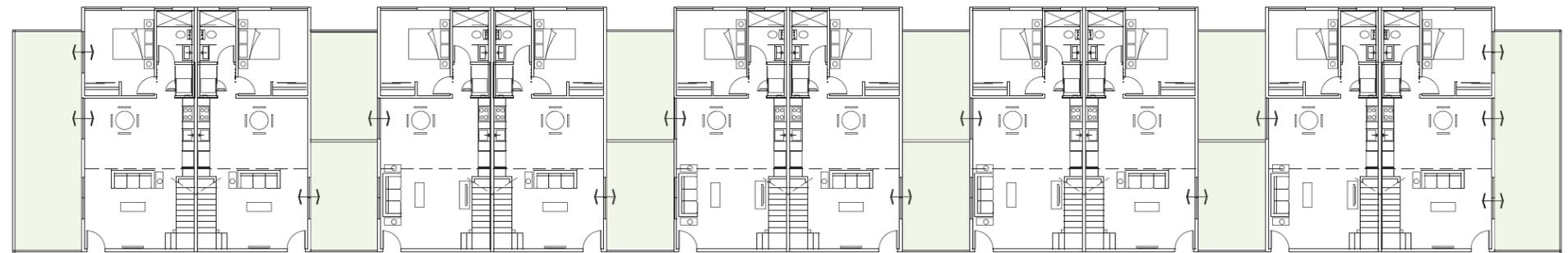
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CITY OF PHOENIX

MAR 27 2020

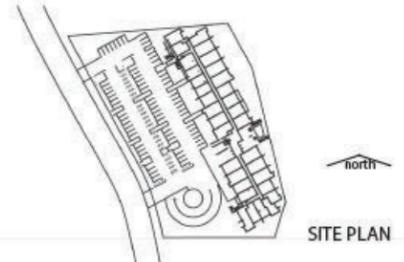
Planning & Development
Department



Plans



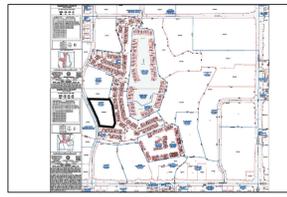
23rd Ave Elevations



CITY OF PHOENIX
 MAR 27 2020
 Planning & Development
 Department

Proposed Rezoning Request for Adaptive Reuse

Property Address:
8125 N23rd Ave. Phoenix, AZ



VICINITY MAP



Existing Office Building:
1st Floor = 31,375 SF
2nd Floor = 31,375 SF
Total GSF = 62,750 SF

Existing Parking:
Surface Parking = 107 Stalls
Garage Parking = 110 Stalls

Site Area:
2.89 AC
125,932 SF

Existing FAR = $\frac{63,750 \text{ SF}}{125,932 \text{ SF}} = .50$

Current Use: CO Commercial Office
Proposed Use: R3A Multi-family

Proposed Project

New Loft Units:
64 = 1 & 2 BD Homes

Required Parking:
Resident: (64) x 1.5 stalls per 1 & 2 BD = 96 stalls
Guest: (64) x 0.5 stalls per 1 & 2 BD = 32 stalls

Total Required Parking = 128 stalls

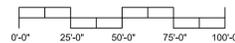
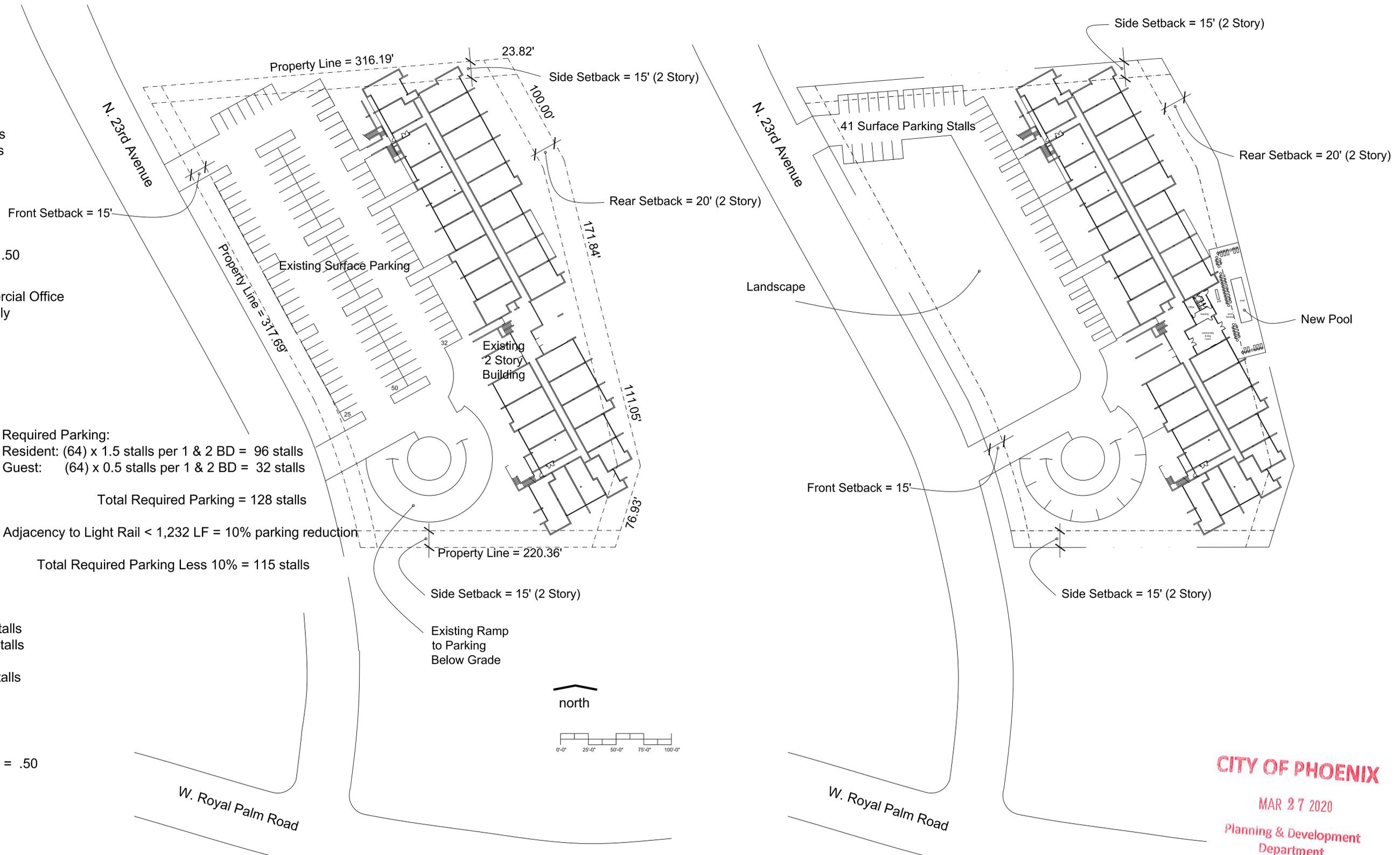
Adjacency to Light Rail < 1,232 LF = 10% parking reduction

Total Required Parking Less 10% = 115 stalls

Proposed Parking:
Surface Parking = 41 stalls
Garage Parking = 110 stalls

Total Parking Provided = 151 stalls

Proposed FAR = $\frac{63,750 \text{ SF}}{125,932 \text{ SF}} = .50$



CITY OF PHOENIX

MAR 27 2020

Planning & Development
Department

From: jeff.spellman@cox.net
To: [Nick Klimek](#)
Cc: "[Kristean Losch](#)"; ken@kenlosch.com
Subject: FW: Z-8-20 8125 N. 23rd Ave - Community Meeting
Date: Friday, June 12, 2020 9:34:26 AM
Attachments: [2020_0505_NEIGHBORHOOD_MEETING-NOTIFICATION_OF_REZONING_REQUEST_Z-8-20.pdf](#)

Nick,

I won't be able to attend the Village Planning meeting where this case will be heard but wanted you to know that we support this project as it supports our goals for revitalization of the VIP area. This is an office building that has been underutilized for many years and we are in great need of quality housing projects. We believe it is an appropriate adaptive reuse project and support approval of the rezoning request.

Thanks,

Jeff Spellman
phoenixvipcoalition@gmail.com
602-499-2038

Violence Impact Project Coalition

VISION: An Active, Safe and Welcoming Community with Vibrant Retail and Sustainable Housing

The Coalition will focus on education, partnerships, outreach and strategic planning within the VIP boundaries of Dunlap to Indian School and 19th Avenue to 35th Avenue.

From: jeff.spellman@cox.net <jeff.spellman@cox.net>
Sent: Monday, May 18, 2020 10:06 AM
To: 'gingermate007@gmail.com' <gingermate007@gmail.com>; 'mikey2kol@yahoo.com' <mikey2kol@yahoo.com>; 'Frank Beaudrot (fbeaudrot@gmail.com)' <fbeaudrot@gmail.com>; 'Bill Brashears' <william.brashears@asu.edu>; 'Mary Obrochta (ocotiloglen@yahoo.com)' <ocotiloglen@yahoo.com>; 'Linda Colino' <piccidesigns@msn.com>; 'hrusk@me.com' <hrusk@me.com>; 'Cindy Graber (graber1@cox.net)' <graber1@cox.net>
Subject: Z-8-20 8125 N. 23rd Ave - Community Meeting

This is a zoning case for adaptive reuse of the 2-story vacant office building on 23rd Ave next to the old LA Fitness. I believe this will be an enhancement to the area, bringing much needed market-rate housing into our area. Positive use will help to displace some of the negative issues we are seeing in that area.

They ask you to join the Facebook page to get the meeting information. I did and here is the information for the Zoom meeting if you can't or don't have time to join the FB group.

<https://us02web.zoom.us/j/89108944623?pwd=TWlhTmk2cTI1WEJxMlVZb3VsR1Bodz09>

[\[1.facebook.com\]](#)

Meeting ID: 891 0894 4623

Password: 006113

Jeff Spellman

phoenixvipcoalition@gmail.com

602-499-2038

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The Coalition will focus on education, partnerships, outreach and strategic planning within the VIP boundaries of Dunlap to Indian School and 19th Avenue to 35th Avenue.

From: Kristean Losch <kris@krislosch.com>

Sent: Monday, May 18, 2020 9:26 AM

To: jeff.spellman@cox.net

Cc: Richard Labonte <richardllabonte@icloud.com>

Subject: Re: Z-8-20 8125 N. 23rd Ave

Jeff,

Thank you for reaching out! I have accepted your request to join on FB and attached is the PDF of the letter to share.

Look forward to meeting you on Zoom.

Thank you,

Kris Losch

Trillium Residential LLC

602.696.5045

From: "jeff.spellman@cox.net" <jeff.spellman@cox.net>

Date: Saturday, May 16, 2020 at 2:00 PM

To: Kristean Losch <kris@krislosch.com>

Subject: Z-8-20 8125 N. 23rd Ave

Hello Kris,

I got your mailing about the upcoming neighborhood meeting for your rezoning request. Your project falls within the boundaries of the VIP Coalition. I registered on FB to join the group. I'd like to encourage our other neighborhood leaders to attend the meeting. Could you kindly send me an electronic copy of the neighborhood notification so that I can share it with the others?

Thanks,

Jeff Spellman
602-499-2038